

International Edition

Free,
electronic magazine
for railroad enthusiasts
in the scale 1:220
and Prototype

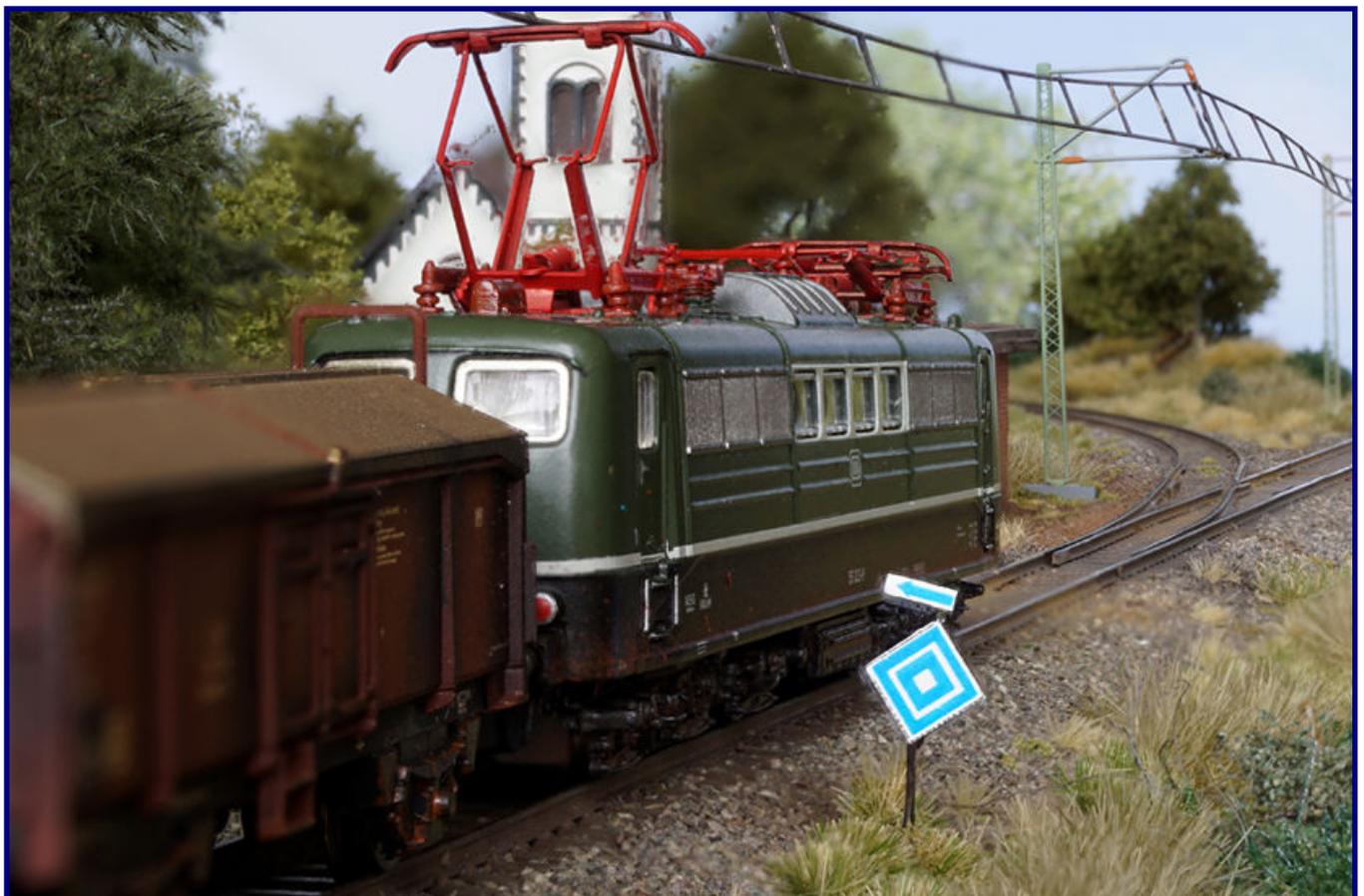
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Trainini

German Magazine for Z Gauge



Electric Locomotive in the History Demolition

Scratch Built Railway Buildings
Service Freight Wagon in Z Gauge

50
Years
of Z Gauge

Introduction

Dear Readers,

It's hard to believe that Christmas is just around the corner! This means that with 2022, our anniversary year will also come to an end in a few days. But we still want to use the little time that remains.

That's why you will find a further part of our series on the annual theme 2022 in this edition. And, if I am honest, I must confess: There could be many more to follow!



Holger Späing
Editor-in-chief

Our model scale has developed magnificently and remarkably. It is so rich in facets, ideas, products, and manufacturers. We could certainly continue this for a few more years. Perhaps some readers will also wonder why their contributions have not yet been included in one of this year's issues.

I have just given the reason for this. Nevertheless, my colleagues on the editorial board and I hope that we have struck a chord with as many people as possible, and that we have drawn a colourful and representative state of our hobby.

Time will tell because nothing is lost with us. What we could not publish without bursting this edition or cutting into our free time too much, we carry forward and plan for future editions.

So, you can be sure we will do our best again in the new year and work to consolidate and build on the reputation **Trainini**[®] has earned in the specialised press. We are proud of this, and it also spurs us on again and again.

You also have a big part in this, as you keep providing us with ideas, suggestions, and proposals as well as photos and contributions. At the end of the year, we would like to thank you once again and ask you to continue to do so and to make and keep our nominal size visible "out there" – even beyond an anniversary year that, unfortunately, saw only a few events.

Two of them are summarised in this issue, and we also look back at everything that happened in 2022. Tinkering fans are catered for by Jochen Brüggemann, who encourages people to build their own building models.

We also look at several selected models. Although not both articles I'm thinking of are explicitly dedicated to the anniversary, it's also worth looking at the past in the context of current offerings.

And, I hope, we can end the year just as colourfully as it began, twelve months ago. Enjoy the reading, use the free days to build or tinker, and stay faithful to our common hobby! On behalf of the entire team, I wish you a Merry Christmas.

Sin-Z-erely,

Holger Späing

50 years of Z gauge (part 8)

Evolution of the 151 Series

In March we looked over the Märklin range of fifty years and reviewed how Z gauge has developed. For some years now it has freed itself from a niche and is finally being perceived as a fully-fledged model railway. Today, we do this with selected models and also see how bumpy the road has been at times.

In July 2012, we had already turned to the model history of the class 103. It was the first electric locomotive in the Mini Club programme and the 40th anniversary of our gauge was a suitable occasion to review its exciting history.

From the announcement in 1974 to the most recent editions at that time, it had undergone many changes that cannot be compared with any other model. We only have to think of the plugged instead of screwed pantographs, which were only available on the very first models of this one series.

But ten years have already passed since our report, which have brought forth further technical innovations. In **Trainini**® 3/2022 we ventured a historical outline of how Märklin's smallest gauge has developed in 50 years up to the present day.



Colourful and very imaginative was the model railway of the seventies, in which also the class 151 for the Mini Club saw the light of day.

We would like to take up this theme and illustrate it with a selected class: We have chosen the class 151, which was the second electric locomotive to be added to the product range in 1975 and which also allows a comparison on the basis of externally identical features.

The ocean-blue-ivory-coloured version (item no. 8858), which followed in 1976, and the orient-red locomotive (8826) from 1991, as well as the chrome-oxide-green locomotive of the Deutsche Bahn AG (88571) from 1996, are left out of our considerations.

They are mentioned here as colour and printing variants, but do not allow a direct comparison with a subsequent edition in the same colour (8826 and 8858) or do not introduce any additional product features (88571) that would not already be in focus with our chosen comparison models.

So, our story begins in 1975: The Märklin Mini Club was three years old and the first 1:220 scale electric locomotive, the class 103 express locomotive, was announced at the Spielwarenmesse (International Toy Fair) the previous year.

It meant a big investment for the Göppingen headquarters, because it is not just another locomotive in the early programme: Pantographs did not exist until now, but for an electric locomotive they are indispensable. The same applies to a catenary system, which was added to the range at the same time.



In keeping with the taste of the times, the new products (item no. 8857) appear in the now very antiquated-looking cardboard box with a wooden look. The care instructions apply to all six-axle electric locomotive models on this chassis: The class 103 is depicted on it, and the ocean blue-ivory 151 has already been included in the item numbers.

Although the prototypes have long since been equipped with more modern SBS 65 single-arm pantographs, Märklin opted for the older DBS 54 scissors pantograph. In retrospect, this reveals itself to be a strategic decision, because almost without exception, all DB unit locomotives are still in service with this pantograph – one for all.

According to the company's self-image, function goes before appearance: The newly designed metal pantographs are not exactly to scale, but robust, mechanically movable and electrically conductive. Accordingly, the stamped catenary wires are also emphatically solid. In the mid-seventies, this did not bother anyone; the Z gauge was on the edge of what was technically feasible.

The decision for the type of pantograph and its electrical function also has an effect on the heavy freight locomotive of class 151, which is also very close to the 103, in some respects, in the prototype.



The new products of 1975 share the running gear with the class 103 announced a year earlier. It adopts the screwed pantograph in the opposite direction to the express locomotive. The printing is outstanding for the time, but still far from today's Märklin capabilities. For example, the space between the class number and the road number is missing.

A common feature that goes beyond the prototype is the shared running gear. Although both locomotives are six-axle and have very similar appearing bogies, the symmetrical axle base on the class 103 is contrasted with an asymmetrical one on the 151. This should protect the superstructure during curved running.

Märklin generously overlooks this and uses the already developed running gear without adjustments. This also applies to the red spoked wheels, which do not want to match either of the prototypes. Economy also took precedence over faithfulness to the prototype, because no model is yet on the road in 1974/75 with disc wheels.

New, not only as a model

So, when Märklin announced its model with item number 8857 in 1975, it was already something special. The two first electric locomotive models in the still young assortment together represent the strongest and newest standard locomotives of the Bundesbahn, and together serve passenger train and freight traffic.

Other representatives are just getting ready to conquer the rails, but at the beginning of the model development, there are no suitable documents available for this. In Göppingen, the people in charge know how to present the everyday life of the Bundesbahn, as well as the modern railway, in an appealing way.

The class 151 has just taken over from its predecessor, the class 150, and production merged almost seamlessly in 1973. The heavy electric locomotive shows a modern face and flaunts tractive power, which is why it is well suited as a figurehead. Certainly, Märklin was already thinking along these lines when they announced the 103.



While the class 103 still has to wait about forty years for this, the 151 gets silver-coloured embossed frames on the front windows right from the start. Typical for the first deliveries is the coupling shown here without a bevel on the outside.

And now the time should have come: Shortly before the express locomotive is delivered, Märklin announces its second electric locomotive for Z gauge at the 1975 Toy Fair. The model chosen was the 151 022-1, which is painted chrome oxide green according to RAL 6020, like all the prototypes delivered to date.

Compared to the premiere model from the previous year, it shows a feature right from the start that will bring its effect a lot closer to the prototype: The frames of the driver's cab end windows are embossed in chrome colour and not just highlighted as engraving.

It also differs from the class 103 (8854) by its bolted pantographs, while the latter is initially still delivered with clamped ones. Since they apparently did not prove themselves, the switch to the mounting method that is still valid today was made.

At the bottom of the chassis the arrow, which indicates the only possible direction of travel in overhead line operation, is again clearly lifted from the black cast block. Since hardly any Zettie uses a functioning overhead line, many asked and still ask about the reason for the arrow, especially since diesel locomotive models that shared the running gear with an electric locomotive also showed it later.

For 15 years, however, i.e., until 1990, the model (8857) would remain in the catalogue. In 1989, one of the latest editions appears with a misprint, the two cab numbers are reversed. The quite few models affected by this find the interest of collectors, at that time, a significant part of the Z gauge community.



The model 8857 was to keep the bright nickel-plated pantographs up to and including the production year 1981. Typical for the model's lifetime were the adjusting screw in the roof and the roof bonnet that was painted contrary to the prototype, which should actually be umbra grey instead of grey aluminium.

However, the green goods train bolides undergo some changes in the course of their product life cycle that can be recorded as product maintenance on a smaller scale: First, Märklin modifies the coupling and only the models of the first years carry the heads without bevelled ends. This change dates back to the 1977 production year.

In 1981, the bright nickel-plated scissor pantographs on this model are phased out. From 1982 until the end, the locomotives leave the factory with black nickel-plated examples, which look considerably more pleasing. 1990 is also the year in which the five-pole motor is introduced. However, the 8857 model will no longer benefit from.

A step into a new century

Many years pass, but the class 151 will never completely disappear from consciousness during this time. Individual variants can certainly be found in the range, just no more green Bundesbahn locomotives. This only changed again after the turn of the millennium.

In 2002, the Mini Club was 30 years old and Märklin does not celebrate this anniversary, but tries to increase its attractiveness with lavishly equipped starter packs. The one with the item number 81862 offers the buyer plenty of track material, no less than six freight cars and a chrome oxide green class 151.



To mark the 30th anniversary of the Mini Club, a new edition of the chrome oxide green locomotive is being released as part of the 81862 starter set. The pack stylistically combines the historical wooden style box with the current product presentation. Photo: Oleksiy Mark

It is marked as 151 023-9, so the road number has been increased by the value 1. This makes it clear that it must not differ from its predecessor. But that is exactly what it does in the best sense of the word.



The goods train included is attractive, as many of the wagons may well be considered desirable models that were not offered individually. And, as in the seventies, the train looks colourful, but without the gaudy appearance of those years. Photo: Oleksiy Mark



The reissue with changed road number shows a recognisable product update: The printing has become even smaller and finer, the roof dormer now wears the correct colour, and, above all, the printed chrome rings at the front improve the 151's face. Photo: Oleksiy Mark

On this anniversary, Märklin can print even more finely than before. And so, all the inscriptions look smaller and finer, more legible through the row and above all the Ege biscuit now looks much better than before. But the appearance of the paintwork has also improved somewhat over the years.



The side view reveals that there is still a lot of work to be done. The red spoked wheels continue to shine at the customer, and the interior is equipped with two incandescent lamps that are no longer in keeping with the times. By the way, the two pantographs were not painted red ex-works, this was done by the owner of the model. Photo: Oleksiy Mark

The new products, which are only available this year, offer a welcome opportunity to be studied down to the smallest detail. The step forward is immediately recognisable on the front sides, where the shiny metallic lamp rings of the prototype can now also be found. If it weren't for the "lobster claw," the photo from the front would be perfect.

Inside, a five-pole motor is now finally working, but the front lighting is still the state of the day before yesterday: incandescent lamps in the plug-in base have long been obsolete in 2002, which does not stop Märklin from continuing to install them here.



The five-pole motor helps 151 023-9 from the starter set to deliver better driving characteristics compared to its predecessor. Here, it is on the move with a goods train transporting fertiliser for agriculture.

It also seems incomprehensible to customers why the class 151 continues to be equipped with spoked wheels, and red ones at that, which are also quite conspicuous in the bogies. The time is not yet ripe for a major leap, Z gauge shows a restrained and slow evolution, not a revolution.

Product care is necessary

We note: The class 151, like many other models, had noticeably developed in the course of its model career. Unfortunately, a lot of this was only related to the exterior, as technically, our scale was at a standstill for a long time at Märklin.

It only picked up speed again after the consequences of the insolvency (2009) had been dealt with and Märklin found calm waters under the care of new and committed owners. As a result, all the important points were defined, prioritised, and finally worked through, step by step.

This can also be seen in the models of this series. First, let's take a look at 151 070-0, which appeared in 1999 in the traffic-red livery and with DB Cargo markings (88572). It can be found in the catalogue until



Three years from 1999, 151 070-0 of DB Cargo (88572) remains in the programme. The spoked wheels remain, but they are now sprayed black instead of red. Angular high-performance buffers and single-arm pantographs, as they have become popular in the prototype, remain a pipe dream.

2002, but the time periods of the offer have thus long been considerably shorter than in the pioneering years.

For the first time, Märklin has dispensed with the conspicuous red wheels on this model and sprayed the inserts black. The choice of red colour for a chrome oxide green locomotive may have been in keeping with the prototype, but we have never seen anything of it on the prototypes, because wheels are inevitably regularly soiled by brake dust.

But our main concern is the spokes, which don't fit the electric locomotive at all. They have remained, but, at least, they are no longer disturbing. The relics with an almost museum-like character also include the plug-in base lamps: for twelve years now, LED has been discovered as a maintenance-free light source for new constructions in Göppingen, but there is still no upgrading of the stock.

The round buffers and the scissor pantographs have also remained and thus the models soon lag behind the prototype. Square heavy-duty buffers and single-arm pantographs will characterise the last maintenance sections of the freight locomotive.

Only the 151 079-1, also in traffic red, but running for Railion (88261), brings the hoped for turnaround in 2015. It now has the buffers and pantographs of the most recent prototypes, and, finally, also convinces with warm white light emitted by light-emitting diodes.

As further plus points, it also features the long hoped-for black disc wheels. And even the printing could be improved: For the first time on this locomotive type, the rubber seals of the engine room bonnets, which are removable on the prototype, are printed as thin black lines.



The red Railion 151 079-1 (88261) is a leap forward in 2015: It finally has disc wheels and LED lighting, and Märklin has also taken up the rectangular buffers and single-arm pantographs retrofitted on the prototypes. The printing quality has also improved once again. Only the announced engine room replica is nowhere to be seen. Photo: Märklin

Only one feature that was explicitly announced does not appear in this variant. We are talking about a replica of the engine room equipment, which customers had expected in the form of a printed sticker insert on the cast block. This version will remain in the programme until 2019.

Let us now take a look back and see the many small steps with which our Mini Club has matured in fifty years from a miniature toy or collector's item for men who may have been somewhat extravagant in some ways to a fully-fledged model railway in tiny format.

However, we must also note that not only modern railways still offer many possible variants with which further editions of the class 151 can be designed.

It is also noticeable that the colour concepts orient red and ocean blue/ivory, which is now becoming much more popular, were underrepresented in the programme.

The technical platform of the locomotive is now up to date. We see this as a sign that the model story is far from over and that further gaps will be closed or previously offered models will be updated to today's standards.



The 162 007 from Hectorrail (88262) is about to be delivered. With it, the visible conversion screw in the roof will also disappear. With the changeover to a bell-shaped armature motor, this model will have gone through all stages of product maintenance. Photo: Märklin

One announcement that fits this pattern is due to be delivered this month, but this was still pending at press time. This is the grey painted 162 007 from Hectorrail (88262), which replicates a locomotive sold to the Scandinavian company.



In the warm autumn light, DB Cargo's 151 070-0 is heading into an exciting future in the service of intermodal transport. The history of the class 151 models will certainly find a few more continuations and produce variants that many Zetties are waiting for.

The changeover screw for the overhead line operation is also relocated to the inside and the drive is changed to the modern bell-shaped armature motor. This means that this model has also gone through all the cycles of product maintenance that exist to date. So the future can come!

Model manufacturer:
<https://www.maerklin.de>

Wagons in railway use

For official Use only!

For the second time this year, we take a look at internal railway transport, but this time only at the Bundesbahn, and with a focus also on two new products from the anniversary year. This article is intended to show how diverse a subject area is that is often misunderstood or not particularly noticed. Maybe we can change that now?

Previously in **Trainini**® 3/2022 we took a look at railway service cars and, in addition to disused passenger coaches, particularly focused on the especially popular rail cranes. In the meantime, NoBa-Modelle has put this topic back on the agenda with its realisations of the Ardelst steam crane 57 to., including protection wagon and optionally additionally available equipment and water wagon.

As these two additions show, the theme has a lot more to offer for the model railway enthusiast, which is why we would like to take a closer look at the service freight cars today. Only, at first glance they seem less spectacular and rather ordinary.



At a road construction exhibition held in Munich in 1934, the 20 t ballast wagon on a road roller demonstrates its basic suitability for road construction when filling in potholes. The Talbot-built self-unloading wagon for track ballasting "Berlin 702 520" is a real classic of railway service wagons. Photo: RVM, Eisenbahnstiftung

In order to be able to distinguish the railway service wagons in their entirety from regular passenger and freight wagons, a definition is first required at this point: These are special railway vehicles that are not used for public passenger or freight transport, but are kept for maintenance tasks and repairs.



The equipment wagon 9 “Kassel 8 303” of the bridge maintenance department Dieringhausen demonstrates in December 1964 at Kassel-Wilhelmshöhe station how vehicles that are no longer able to cope with the traffic demands are put to a second use as service wagons. Photo: Reinhard Todt, Eisenbahnstiftung

Their field of application is consequently the maintenance of railway facilities in all of its facets, from buildings to tracks to overhead lines. They are subdivided into railway service wagons and service freight wagons. Rail service wagons are used for a specific purpose and not for the transport of goods. They include, for example, accommodation carriages, doctors' carriages, calibration carriages, auxiliary equipment carriages, material carriages, or workshop carriages.

Service goods wagons carry the necessary transport goods for service purposes, including to construction sites along the route. Their typical loads once included, for example, gas for lighting the locomotives, carriages and stationary lanterns, water for steam cranes, ash and slag produced in the steam depot.

They also included refuse (especially station wagons), construction machinery, or the ballast for track construction sites. In the latter function, they used to be called work wagons. Whereas the transport of locomotive coal once attracted no one's attention, the situation was somewhat different for bunker oil C (heavy heating oil).

This was carried in special service tank cars that were steam-heated and insulated to keep the refineries' waste material liquid. Similar to tar, it only becomes liquid when heated. At that time, there were no customers other than the Federal Railways, which is why it was possible to deduce the intended use of the contents from the external shape of the individual wagons.

Often, all railway service wagons are of such construction types, that can no longer be used or approved for public transport due to their age and outdated construction characteristics. In some cases, they are also converted for their new tasks.

These include, for example, side windows closed with metal sheets on earlier conversion wagons, subsequently inserted roller shutters, removed wagon crossovers, or also new transfer windows in covered freight wagons, which are intended to provide an incidence of light when working inside.



This stake car Rmms 33 with the road number 432 888 was photographed in Bremerhaven in September 1960. At that time, it was no longer common that it was still used with stanchions. Later, representatives of this type were still used as service freight wagons or railway service wagons into the nineties. Photo: Reinhard Todt, Eisenbahnstiftung

Railway service car as a model

We have already seen classic railway service cars several times in the Märklin programme, among them several times above all disused conversion cars, Donnerbüchsen and also the former Württemberg two-axle cars.

However, it is noticeable that all of the models had Era IV markings and were painted in RAL 5020 ocean blue. This became common at the DB in 1974/75, when the new colour concept was introduced.

Prior to that, railway service cars, regardless of their origin as freight or passenger cars, wore a paint scheme of RAL 6020 Chrome Oxide Green. Märklin has so far only used this colour occasionally for G 10, the Ardelt crane and associated crane protection wagons.

For the 1zu220-Shop, an "Oppeln" manufactured by FR Freudenreich Feinwerktechnik in a goods wagon pack (item no. 49.019.04) wore the smart chrome oxide green as an equipment wagon. Perhaps this seems too risky for the manufacturers so far, as former passenger coaches hardly stand out from their



In the “Epoch III freight wagon pack” (item no. 49.019.04) for the 1zu220-Shop was this “Oppeln” as a chrome oxide green equipment wagon. It is nice to look at, but it may be doubted that such a widely used freight wagon, still regularly used in international service, had migrated from public transport before 1974. Photo: Jörg Erkel

siblings in public transport. The only exceptions to the uniform livery were once the black air-conditioned snow ploughs.

The occasion for this article is the latest special model of the 1zu220-Shop, which we had already announced in **Trainini®** 3/2022. Three quarters of a year ago we were only able to show the sample illustration of the planned addresses.

In the meantime, the equipment wagon 631 (49.342.31) produced by FR Freudenreich Feinwerktechnik in a one-time edition of 40 copies for the anniversary “50 years of gauge Z” has long been sold out: This took less than 24 hours.

The ocean-blue wagon follows a documented prototype and is a modernised Bremen type war goods wagon that was retrofitted with a side window for its purpose as a railway service wagon. In this way, it is not only clearly distinguishable in colour from regular covered wagons.



In the seventies, the “Oppeln” was taken out of service; its use as the ocean-blue workshop car 611 (49.334.211; photo on the left) only becomes credible as a result. The prototype of the equipment car 631 (49.342.31; photo on the right) is in any case proven by photos. This special model was released on the occasion of “50 Years of Z Gauge”.

The workshop wagon 611 (49. 334.211), which perfectly matches it, has the same origin as a model and comes from an earlier edition, cannot boast such features. It is also painted ocean blue, but otherwise



The chrome oxide green steam snow blower from Märklin (81360) also belongs to the railway service vehicles. Pushed by a class 50 trailing tender locomotive, the Insider Club members had the opportunity to purchase it in 2001.

remains unchanged externally. This wagon is based on the earlier Oppeln type, whose major phase-out was in the seventies.

No railway service car represents another model that we want to mention here. It fits perfectly here in terms of the season and follows the earlier chrome oxide green regulation paint scheme that we have already mentioned. We are talking about the DB's Henschel steam snow blower, which Märklin had offered together with a class 50 steam locomotive with tender as an insider model in 2001 (81360).

Before that, Westmodel already had this model in its range, but from large-scale production it probably reached a much larger customer base despite compromises with the bogies.

Service freight wagon in model

Let us now also take a look at the service freight cars. We had described them as seemingly unspectacular and we would like to tie this to a rather prominent example. In the inventory of almost every long-time Zettie, but rarely perceived as a wagon of this purpose, is the short Talbot ballast wagon.

It is a classic in the Märklin programme: it was listed in the catalogue under item number 8624 from 1981 to 2008, i.e., for almost thirty years. During this period, it was always labelled for the DB and bore a UIC number that assigns this model to Era IV.

Only a few will have noticed that "Stg" (Stuttgart) is written for a maintaining railway directorate, and that the wagon bears the designation "ballast wagon" instead of a class code.

In fact, this is a type that was always used as a service freight wagon and never ran in public transport! It is therefore predestined to be added to construction trains on the model railway.



The Talbot ballast wagons (8624), which appeared in 1981, were very detailed and even had movable round slides. They are true classics among the service freight wagons, as they were once built exclusively for this purpose (cf. photo on page 15). Here they even carry matching and particularly realistic looking load inserts of the Z-Freunde Saarpfalz.

The model was a “good throw” on the part of Märklin at that time, because the conversion was done correctly and to scale. The fine details include rivet reproductions on the frame and the fine rotary slides, including operating levers, which were not oversized despite their mobility. In 2011 they returned to the programme together with (a too roughly realised) load as a five-part pack for the DRG (86244).

The fuel oil tank cars of the Bundesbahn are often misinterpreted in a similar way. Connoisseurs also notice here that they also do not bear any class marks, which would be enormously important for cross-border traffic because of the design features.

Another indication is the fact that they have the railway's own addresses and no registration number for a private wagon. Tank wagons for public transport were never in the stock of the state railway in West Germany. They used to belong to the mineral oil companies and later to the well-known rental companies.

So, there had to be something more than just the exterior shape special about the models that appeared in 2005. They were offered as an MHI special series in a pack of three for Era III (82060) with addresses of BD Kassel. Two years later, these moulds served the Insider annual coach (80317), which had DB biscuits and was marked for Era IV. Incidentally, the DB only had 27 units of the heat-insulated coaches produced for its own use.



Classic service freight cars in the Märklin programme are also the insulated tank cars for transporting heavy heating oil (“bunker oil C”) that was burned in steam locomotives. In 2005 they also made their way into the Mini Club programme (82060).



Provided with operating traces, the Epoch IV version (80317), which was issued as an Insider annual car in 2007, runs at the front of a tank car train.

We would like to address one last original here, which was a good choice in relation to the chosen prototype: With the 2022 spring new products, Märklin announced a DB Rmms 33 rail service stake car (82133).

These vehicles were once numerous and also very popular for such services. Labeled for Era III, the wagon lacks only a suitable construction train in the current programme that it could complete. It was pleasing that in the delivered model all stanchions were enclosed separately for insertion and the end ones were not glued firmly to the body.



Fresh out of the box, the Rmms 33 (82133) used as a service freight car is not yet convincing. Its load requires reworking.

This is to be welcomed insofar as these wagons were used by the Bundesbahn early on, and then, almost without exception, as ordinary low-sided wagons. The enclosed stanchions are therefore also dispensable here.

Hopes were raised by the enclosed load insert with ballast pile, sleepers and box, which is made of coloured resin casting. A shortcoming that became apparent with delivery, but was not clearly visible in the official product photo, is the thickness of the floor base due to the casting.

The insert, which is a few millimetres shorter than the loading area, reaches up to the upper edge of the side wall, and in some cases it also seems to protrude slightly, because resin also tends to warp slightly during hardening.

In addition, although the ballast pile and the wooden box have been colour-contrasted, the base has retained the grey colour of the chosen primer. This is also disturbing and not prototypical, because in the prototype the sleepers would rest on the wagon floor and not on a concrete slab.



This comparison of the factory condition (left) and the reworked insert (right) is intended to enable a comparison: The load now sits much lower in the wagon and no longer looks over the edge.

The load, which is certainly very interesting for a construction train, shows slight weaknesses, which are the result of economic cost pressure. However, they can be remedied quite easily, as we would like to show here on the basis of our photographs.

First, we stretched sandpaper on a flat surface and moved the insert back and forth on it so that its underside was evenly sanded. If you plan to do this as well, remember to wear a dust mask, so that no sanding dust gets into your respiratory tract!

We cannot give a fixed measurement for the material to be removed, because we have approached it by fitting samples until the result corresponded to our wishes. In addition, as already mentioned, the plate may also have a slight and hardly noticeable bulge, which does not have to be remedied by a hot water bath.

In the following step, we also optimised the colour of the load insert. RAL 8012 red-brown is the colour of the superstructure, which we have as an Oesling model construction product and in this case applied with the finest possible hair brush.

It is important that even the joints between the sleepers can be reached with a little paint without smearing or staining them. Also along the edge of the ballast pile and the wooden box, the wagon colour was given to the base surface until it visually seems to form one unit with the wagon floor.

Finally, we “secured” the box with black tension bands. It has two circumferential, fine cracks, which we highlighted with matt black Revell paint from the Aquacolor range. Since our red-brown dries silk-matt and the box also has too much gloss, we finally fogged over the cargo with Noch's adhesive and fixing spray – done.

We were also able to solve the problem of finding a suitable train for the job: Without further ado, we transferred the optimised load to the Klm 441 low-sided wagon from the Märklin pack 87761. This model is identical in construction, but labelled for Era IV.



Even from a higher perspective, the load insert, which has been reworked with sandpaper and paint, now looks much more realistic. It is now loaded on the identical Klm 441 low side car from the new 87761 construction train set, which has been supplemented here with the 611 workshop car and the 631 equipment car from FR Freudenreich Feinwerktechnik.

We liked the refinished insert better and found it more suitable than the bare rails without any trace of rust that came with this pack. Moreover, new rails are regularly 30 metres long and therefore cannot be run on a single stake wagon. There are many good ideas, sometimes it is just a matter of implementing them consistently.

Base model manufacturer:
• <https://fr-model.eshop.t-online.de>
• <https://www.maerklin.de>

Special model provider:
• <https://www.1zu220-shop.de>

Materials used for reworking:
• <https://www.noch.de>
• <http://www.oesling-modellbau.com>
• <https://www.revell.de>

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Build your own structures (Part 1)

Railway Building Construction with available Resources

The range of Z gauge building kits was quite limited, even beyond the year 2000. Layout builders looking for some variety had to rely, therefore, either on modifying them or completely building them from scratch according to individual templates. For more than 40 years, Jochen Brüggemann has often relied on his own skills together with “on-board resources”.

By Jochen Brüggemann. When I started building my current layout in 2002, I was still unfamiliar with lasercut kits; so for building I decided to use the materials and methods I had already been using since 1980 on my previous layout: polystyrene, cardboard, also leftover packaging material, and both kits and partial or complete self-built structures.

In the meantime there is a considerable, and constantly growing, range of (very) good lasercut model kits on the market. However, I have found that placing polystyrene and lasercut models on the same layout does not make for the best visual effect.



Loco 044 671-6 drives to the turntable at its home station Ottbergen on 26 May 1976 and passes the locomotive control tower, which served as the prototype of Jochen Brüggemann's scratch-built model. Photo: Wolfgang Bügel, Eisenbahnstiftung

This is especially true in the foreground, where details tend stand out more than compared to other areas of a layout. And, to completely replace my existing polystyrene buildings with cardboard ones was (and is) out of the question for me – I like my own buildings too much for that.

To avoid a break in style, I thus do largely without cardboard kits and continue to use the materials that I have tried and tested for years. Partly according to my own ideas, partly according to real prototypes, my buildings, partial and complete structures, are created at the dining table and with “home remedies”. The most recent building projects are a locomotive control building and a signal box, both largely made of polystyrene (PS) and with a brick wall look.

Building the locomotive control tower

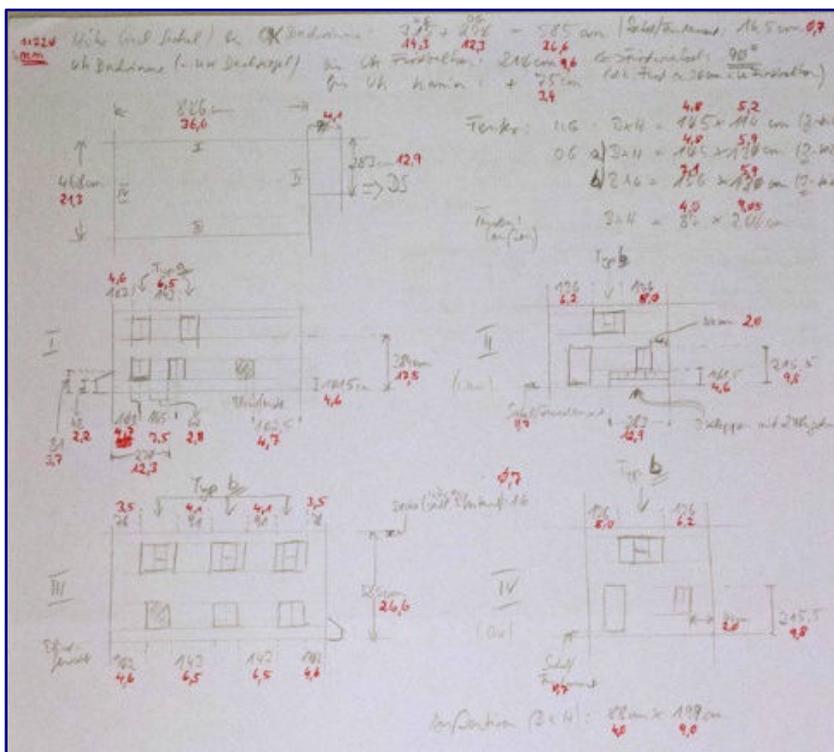
Planning is the first step before the start of construction. For example, I use the following as a basis:

- scaled (original) plans of a building,
- self-designed ground plans and side views based on photos, sketches and dimensions of individual parts of a real prototype, or
- building plans inspired by existing models.

In this case, my goal was a (slightly simplified) model of the locomotive control tower of the former Ottbergen railway station. The cuboid, two-storey brick building had an entrance door on each of its narrow sides.

One of the two entrance doors formed the access to the recreation room (on the ground floor) for the turntable attendant; the other led (immediately to the right on the ground floor) to another recreation room for staff, also on the ground floor, and straight ahead via a staircase up to the upper floor, to the office of the locomotive dispatcher.

Fortunately, I was able to use scale drawings, some with original dimensions, from a railway reference book, the printing of which is so accurate that additional dimensions can be determined from the drawings with a sliding gauge and then converted to our scale.



Sketches of the parts to be built with the original dimensions (pencil) and the converted values for the scale 1:220 (red).

Based on this information, I made rough sketches of the individual sides of the building with their window and door openings on a sheet of paper and converted these measures to the corresponding dimensions on a scale of 1:220.

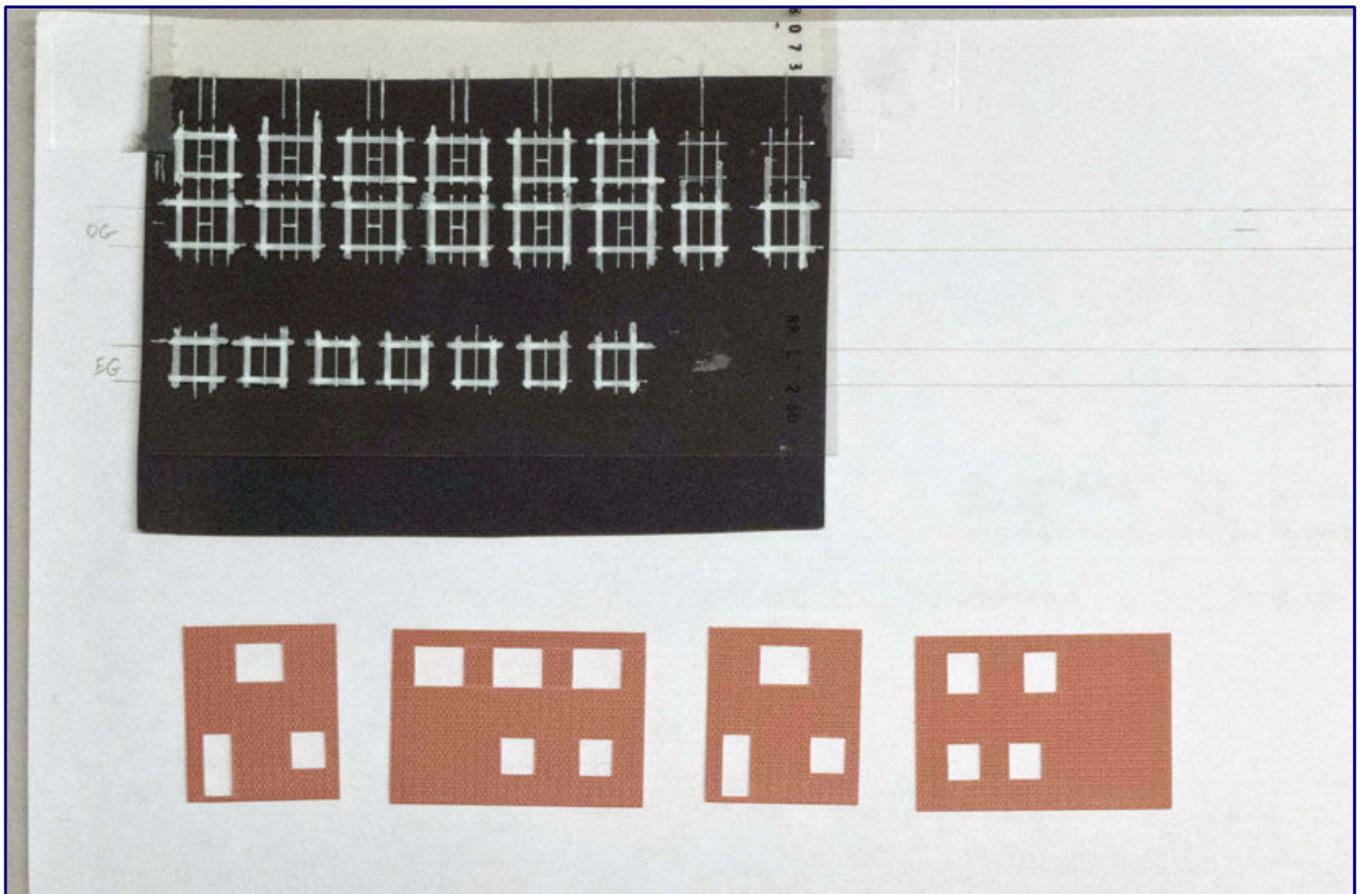
In the original, the hipped roof of the building had a slight horizontal bend in the lower area. The reason: The supporting roof beams on the concrete ceiling rested exactly above the brick walls, but the concrete ceiling overhung on the outside. So, the roof beams in the lower area were extended at an angle so that the tiled roof reached over the outer edge of the concrete ceiling.

In this respect, I have simplified my model plans somewhat: the model lacks this kink in the roof, so that the roof sides each form smooth surfaces from the ridge to the roof edges. And, I increased the ridge angle from 90° to 100° in order to keep the approximate height proportions from the house to the roof. After the planning phase, it was time to get to work.

Since I don't have an equipped precision mechanic's workshop, I relied on my tried and tested on-board tools: Soldering iron, fine pliers, files and tweezers, a calliper, a pencil punch, fine drills (from 0.3 mm), a scalpel with different interchangeable blades, ink pens (0.3 and 1 mm, e.g., from Rotring), plus white drawing ink, sandpaper (220 grit or finer), scraps of polystyrene packaging material, cellophane tape, copy paper, and fine brushes.

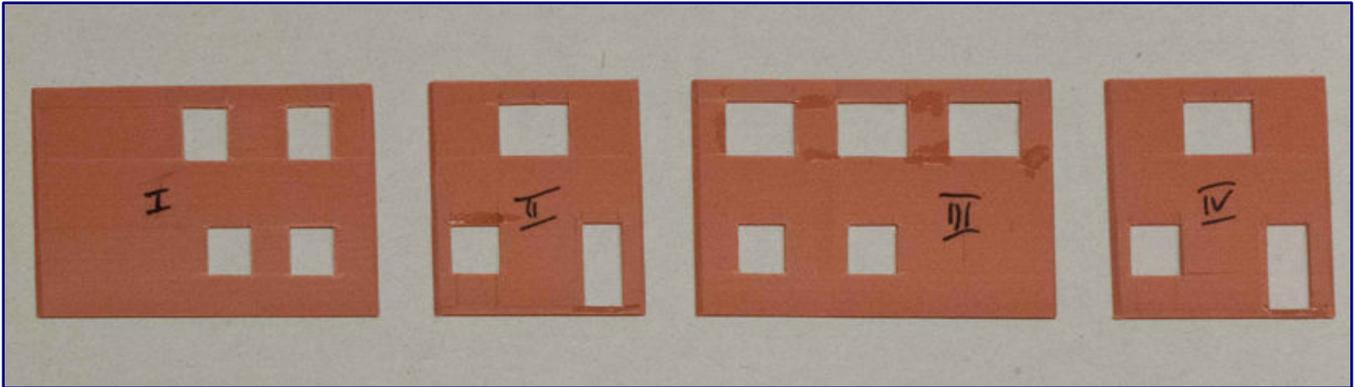
For the outer walls I used a 0.75 mm thick polystyrene structure plate with brick wall look from Schulcz (art. no. 380-26-0001 at Architekturbedarf.de); for smooth surfaces I used polystyrene plates (0.3 mm and 0.5 mm thick) from an architectural supply shop and for the roof Kibri tile roof plates (pantile roof plate for Z gauge; 36920).

The source material for the windows were former b/w photographic negative plates with a relatively thick base. These came from my collection of materials, which I rescued from the waste many years ago, and chemically developed and fixed in their unexposed state.



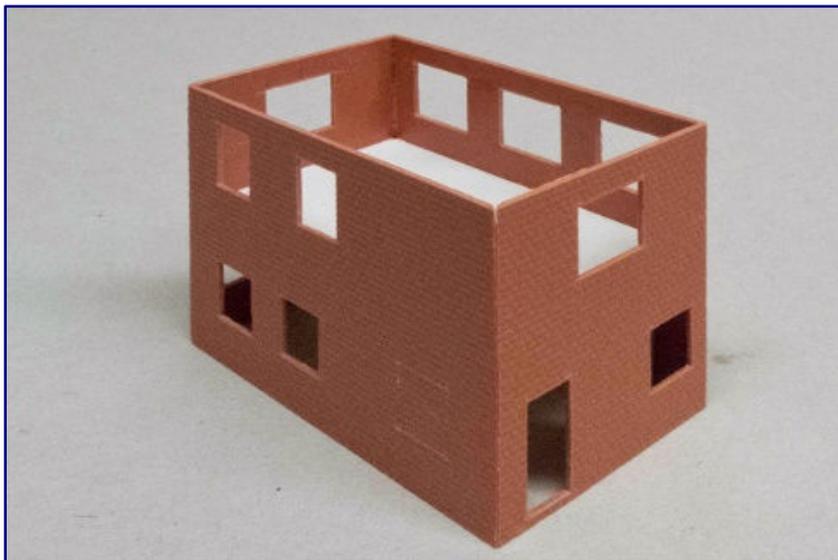
The side walls of the locomotive control tower are marked on the inside so that they can be joined together in the correct sequence. They are now ready for assembly.

For various add-on details I used left-over parts from old Kibri kits and Evergreen profiles. For the lighting I used type 0603 SMD-LEDs (source: Hightech Modellbahnen), a universal diode of the type 1N4148 and different carbon resistors (0.25 Watt, 3.3 kΩ resp. 8.2 kΩ). The electrically conductive connections inside



The side walls of the locomotive line building are marked on the inside so that they can be joined together in the correct sequence. They are now ready for assembly.

the building were made with Busch silver conductive lacquer (5900). Thin plastic glue, white glue and Busch grass glue were used as adhesives; for the colour work I used water-based and completely matt-drying acrylic paints (water-based).



Shortly afterwards, the "shell" of the locomotive management building is in place: the side walls are joined together, a false ceiling is inserted.

I began by drawing a scaled outline of all the windows of the building on paper, including the mullions. I had to take into account that the windows within each floor are of the same height, but that those on the upper floor are higher than those on the ground floor.

Then, I drew the four sides of the control tower, with all window and door openings, on the back of the polystyrene brick wall structure plate, and, if possible, in such a way that the window and door lintels end with stone layers.

Minimal deviations from the prototype could not always be avoided; in such cases, I made a slight height adjustment by eye in order to achieve an approximately true-to-scale storey height again.

ment by eye in order to achieve an approximately true-to-scale storey height again.

One after the other I cut out the window and door openings with a sharp scalpel and then the complete wall parts from the board. I smoothed the cut edges with a file and sandpaper, and also sanded the side edges of the wall parts to a 45° mitre so that these edges could be joined as seamlessly as possible to form 90° corners.

In some places it was not possible to avoid tears in the embossing plates; I carefully repaired these on the inside with plastic glue. Two windows that were bricked up in the prototype were indicated on the outside of the wall parts by scoring them with a scalpel.



These two photos show the locomotive control tower with finished exterior and interior painting as well as the inserted windows and entrance doors. The first two interior walls on the upper floor have also been installed. On the left photo one can see the door to the turntable attendant's lounge on the ground floor, on the right the entrance to the locomotive control room.

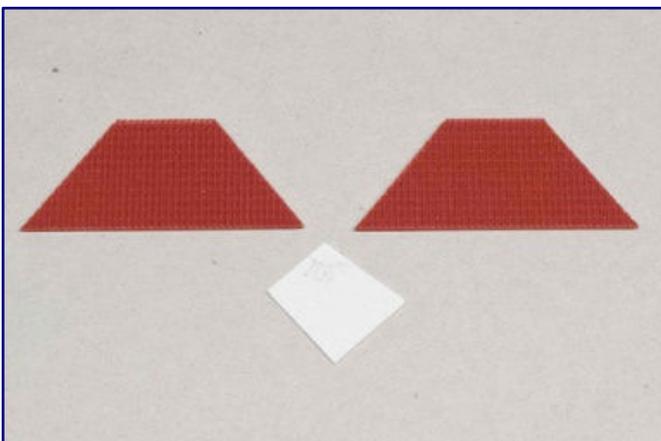
On the paper with the window template, I secured a sufficiently large photo negative plate (with the shiny side facing up) with two small pieces of Scotch tape to prevent it from slipping and then drew the window frame edges and the muntins with the 0.3 mm drawing pencil, using white ink.

Then, I gradually widened the edges of the frame with the 1.0 mm pencil and the bars with the 0.3 mm pencil. Between each step, the ink had to dry well; then I checked the effect of the windows by placing the corresponding side piece on top and touched up, if necessary. A piece of black paper, pushed between the window outline and the photo negative plate, made it easier to assess the quality.

Walls, roof and ridge tiles

I carefully glued two adjacent wall parts (i.e., one transverse and one adjacent longitudinal side wall) together with thin plastic glue and carefully aligned them (horizontally and vertically) on a straight surface with the help of a triangle ruler.

Then, I added the other two wall sections one after the other in the same way and constantly monitored the correct fit. Next, I cut out a rectangular piece of 0.5 mm thick polystyrene board that fitted exactly in place as a ceiling between the ground floor and the upper floor.

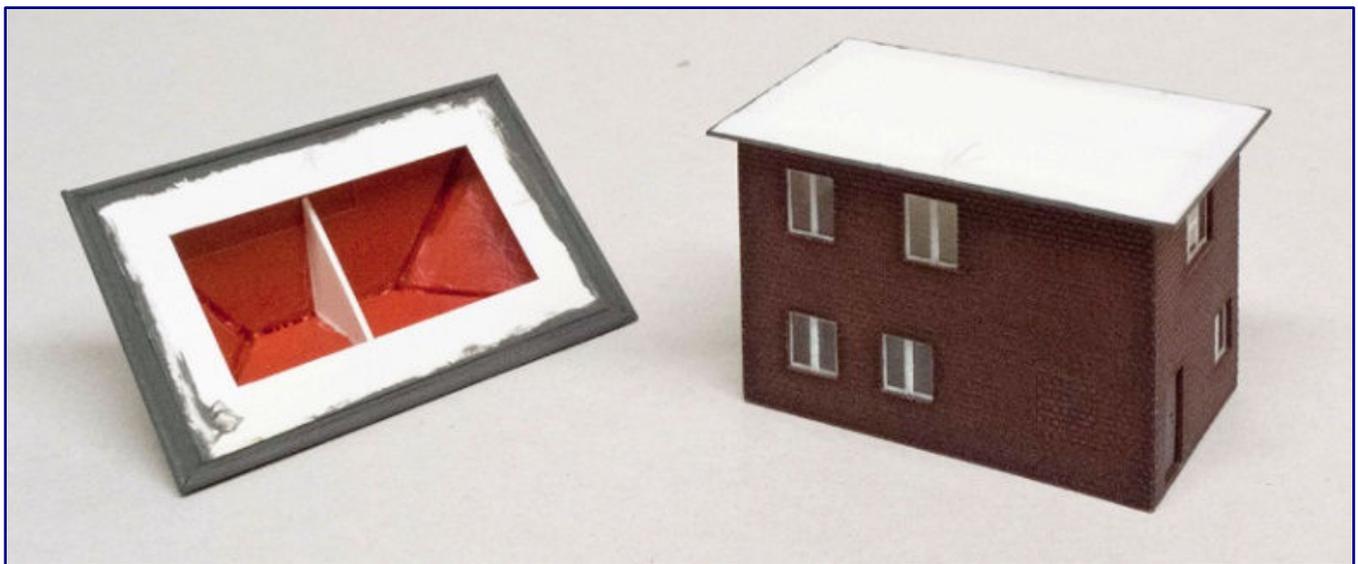


Between the two longitudinal roof surfaces lies the polystyrene template with the 100° ridge angle (photo left), which is glued in as a support on the inside (photo right).

The result was a cuboid shell with right angles horizontally and vertical walls and edges. I also cut out a piece of 0.3 mm polystyrene board of the same size as the intermediate ceiling, in preparation for a later step.

The shell was painted on the outside (including window and door niches) with slightly diluted acrylic paint (my own mixture of “new bricks”); for the carved, “bricked-up” windows I lightened the colour a little. After drying, I covered the shell side by side with an anthracite-coloured glaze (acrylic paint diluted with water).

I painted the interior surfaces of the ground floor black with acrylic paint. Then, I painted the side walls on the inside of the upper floor with white acrylic paint, taking care that no white paint was visible from the outside at the window edges; the floor of the upper floor was coloured dark grey with acrylic paint.



The concrete slab (under the roof) rests on the body of the building for a positioning test. In the meantime, the finished roof section has also received its colouring, as can be seen on the edges of the underside.

Then, I cut out the prepared windows from the negative plate with a little overhang, and, using small drops of plastic glue at the corners, glued them to the inside of the walls and aligned them as accurately as possible.

A longitudinal wall for the upper floor was made from 0.5 mm thick polystyrene, just 0.5 mm lower than the upper floor (interior), provided with a door opening and the cut-out door glued on when open.

The wall was painted white, the door in light brown; the door frames are also indicated in the same colour. I glued this wall, slightly offset to the side, lengthwise on the upper floor.

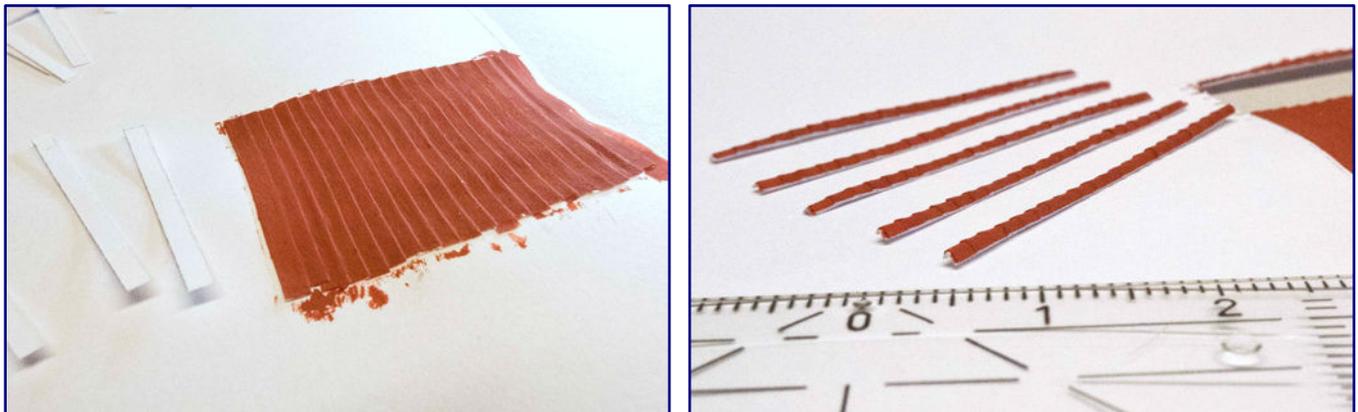
In addition, I inserted a short wall piece of the same height and material thickness transversely between the outer and inner wall according to the plan drawing, resulting in a small windowless room. The access door from the office was indicated by a rectangle painted in light brown.

On a leftover polystyrene piece, I then drew two matching rectangular pieces for the two entrance doors on the ground floor, traced the lines with a blunt back of scissors and divided the areas into three roughly equal parts with two blunt cross lines each. These doors were cut out with an allowance, painted dark grey, with door handles painted on in black and glued in front of the door openings from the inside.

To match the 0.3 mm thick polystyrene sheet, but with 2.7 mm overhang on all sides, I made a 0.5 mm thick polystyrene sheet and glued it to the thinner sheet so that an even overhang remained. Placed on the upper floor with the thinner panel facing downwards, but not yet glued, this panel construction replicates the concrete ceiling under the roof structure.

The thin slab part fits exactly into the upper floor interior as a guide and ensures an even overhang of the thicker, upper slab part. In addition, I made another slab just as big as this concrete slab, but made of only 0.3 mm thick polystyrene, to use it as a loft later. I cut out the centre of this slab.

This attic slab formed the basis for the following roof construction. First, the dimensions of the two hipped roof longitudinal side surfaces were determined, in this building by a trigonometric calculation.



The basis for the ridge tiles is created by overlapping strips of paper, which are then covered with a brick-coloured paint. The desired ridge tiles are then obtained by simply cutting 1.5 mm narrow strips from this sheet in and bending them lengthwise into in a U-shape.

In doing so, I calculated a slight, even roof overhang over the attic slab. In earlier buildings, however, I had already found out the dimensions and shapes for complicated roof forms first by experimenting with thin cardboard.

Then, I cut and sanded the polystyrene roof panel, taking into account the tile orientation and structure, so that the lower edge of the roof was flush with a row of tiles. Then, again on the back of the PS panel, I drew the two long sides of the roof and cut them out with a sharp scalpel.

The ridge edge and the two sloping edges were smoothed and mitred (approx. 45 - 50°). I glued the two roof panels together with their ridge sides; a matching triangle made of polystyrene, attached from below across under the ridge, ensures the ridge angle of 100°. While the glue was drying, I had to check continuously whether the two halves of the roof had been fixed in the correct position.

Only then did I measure the two narrow sides of the hipped roof, transfer them to the roof panel, cut them out, sand and fit the edges and finally insert them. After these glued seams had also dried, I sanded the bottom of the roof straight on sandpaper and glued it to the roof panel.

A test run on the body of the building (with the concrete ceiling in place) confirmed the correct position of the roof. Next, I now glued four matching pieces of 1.0 mm wide polystyrene profiles with a semicircular cross-section (from Evergreen; distributed by Faller) as guttering at the bottom of the roof edges, with a good 0.5 mm overhang.

Finally, I painted the roof with slightly thinned acrylic paint (own mixture “new tiles”), and the gutter, as well as the underside of the roof, including the outer edges with medium grey acrylic paint. The sides and the projection of the concrete ceiling visible from below were also painted with the same grey colour.



With the ridge tiles glued on and a first weathering wash applied, the building already takes on a very realistic appearance.

As I have not found any suitable ridge tiles for the Kibri tiled roof panels in the accessories range so far, I had to create my own from copy paper. To do this, I glued about 20 strips of paper, each 2 mm wide, exactly parallel to each other, and overlapping the long sides by about 0.3 mm, to a sheet of paper with white glue and painted them with slightly diluted acrylic paint.

Turning the paper 90°, I cut 1.5 mm wide strips. I placed each of these strips individually backwards on a piece of thick cardboard, pressed them in lengthwise on the (white) underside with the back of a pair of scissors (not scored or cut) and then bent them open in a channel between the thumbnail (on the white underside) and the tip of my index finger.

Cut to length, these ridge tile strips were placed on the ridge and the four sloping roof panel edges with white glue and painted with tile paint on the white edges. Finally, I covered the four tile roof surfaces one after the other with the anthracite-coloured glaze.

Interior and finishing touches

From the outset, I had planned to install lighting only on the upper floor of the locomotive control tower, namely for the locomotive dispatcher's office and the vestibule. And because the windows of the locomotive dispatcher's office allowed a partial view, I had also planned a simplified interior.

I took off the roof and the concrete ceiling and made a partition between the office and the vestibule out of 0.5 mm thick polystyrene sheet. At one end of this wall I painted both sides with imitation doors made of paper in medium brown; the largest remaining part I used for a cut-out as a hatch. In the prototype, there was a triple divided window here, in the lower part of which a hatch was placed in the middle.

I inserted this partition between the longitudinal wall and the outer wall between the 2nd and 3rd upper floor windows. A U-shaped desk top and matching side panels were made from 0.3 mm polystyrene sheet, as well as a simple chair from three small polystyrene pieces.

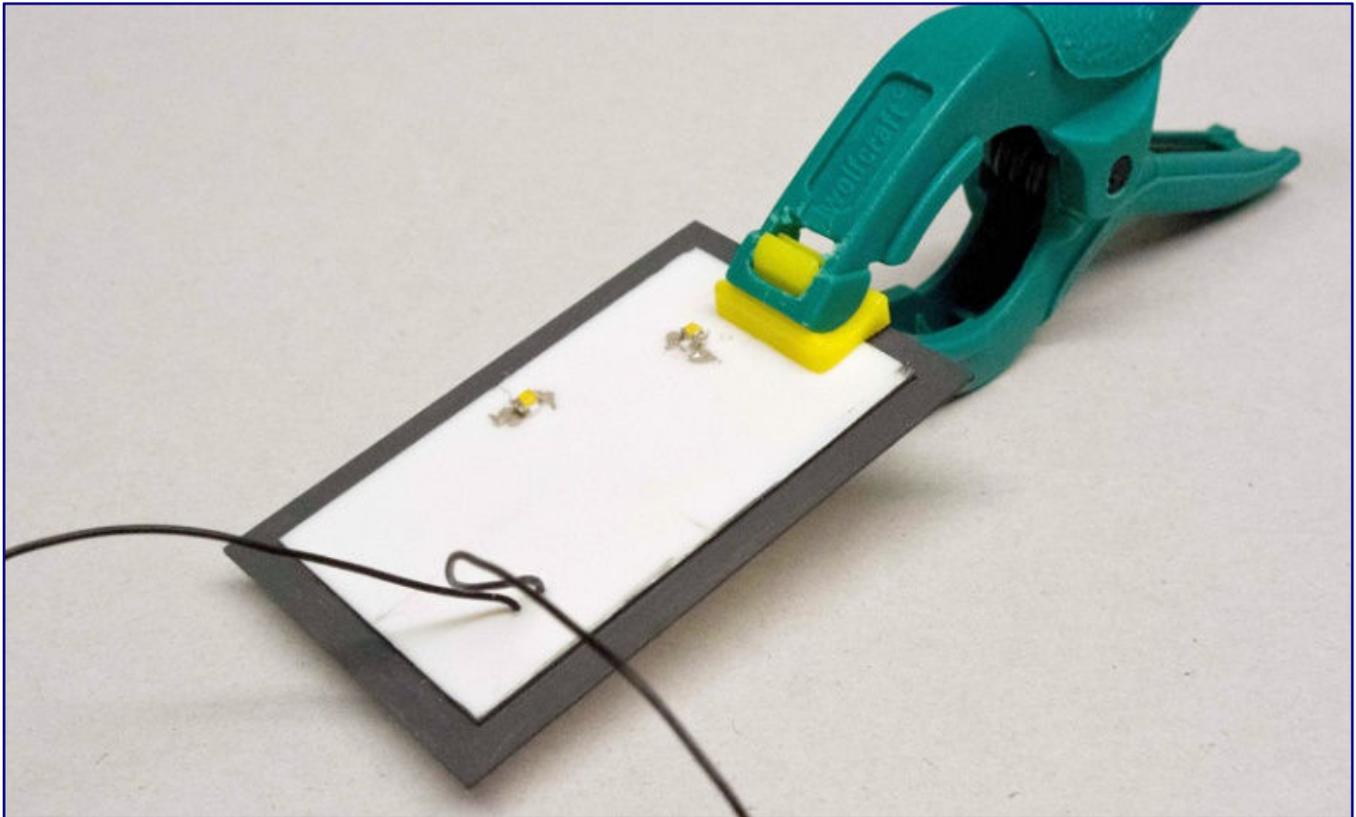


The view into the upper floor of the control tower shows the interior. In the prototype, the area between the windowless room in the foreground and the open door provided room for the staircase.

I painted this furniture light brown. From various pieces of polystyrene, some of them less than 1 mm in size, I made a grey telephone and a few files (black with white spots on the back), and a few pieces of white paper with fine black dots form the office documents to be filled.

After fixing these utensils to the desk surface with Noch grass glue, I glued the desk in the office (also with Noch grass glue). From a Preiser bulk pack of unpainted miniatures (88500) I chose a sitting railway employee, painted him with acrylic colours and placed him on the chair in front of the desk.

On the underside of the concrete slab, I glued two 1 mm² pieces of polystyrene so that one was above the desk and the other was in the middle of the vestibule, and glued a 0603 SMD LED to each of them.



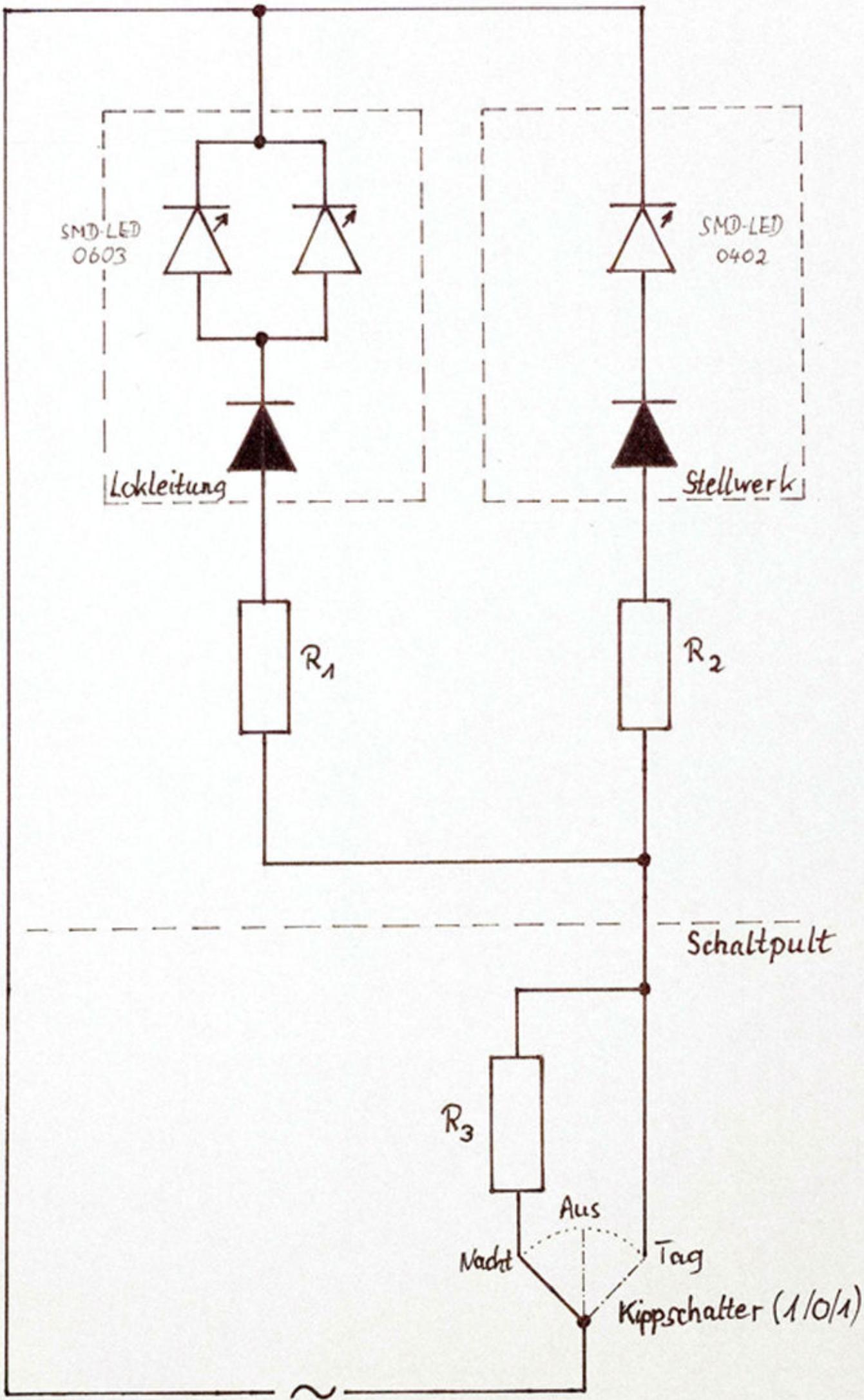
The two LEDs of the ceiling lighting are installed on the underside of the concrete ceiling. Their connecting wires are also visible. The common protective diode is on the opposite side and is therefore not visible in the room.

Near the LED transverse sides, a 0.3 mm hole had to be drilled through the concrete slab on both sides with a pin vise drill. I drilled two more holes, but 0.6 mm wide, in the area of the concrete slab that is above the windowless room of the upper floor.

Now I soldered a universal diode of the type 1N4148 and a piece of brass wire to a piece of 0.5 mm thin stranded wire each and fixed it on the upper side selectively with white glue. The two free stranded wire ends, stripped and pre-tinned, were pulled down through the two larger holes.

Using a test circuit with a protective diode and a ballast resistor of at least 2.2 k Ω , the forward direction was determined for each of the two LEDs. The electrical connections of the two LEDs, one to the protective diode and the second to the brass wire, were made, according to the forward direction, with short, thin pieces of enamelled copper wire, the ends of which were scraped off with a blade. The conductive connection was made with silver conductive lacquer from Busch.

continues on page 36



This was followed by another LED function test. Two 1 mm holes in the floor of the windowless room later formed the access holes for the two connecting wires from the upper floor to the ground floor.

In the next step, I pulled the two connecting wires from the upper floor through to the ground floor, aligned the concrete slab, and applied some Noch grass glue to the upper edge of the outer walls. Then, I lowered the slab onto the outer walls, making sure that it was in the correct position: the grey overhang of the slab should lie flat on the wall everywhere and be evenly wide on all sides.

I carefully pulled the two strands down and pressed the plate lightly into place. Then, I put the roof on, also with grass glue, and aligned it exactly with its base plate on the concrete.



The locomotive control tower is ready to be placed on the layout: the roof has been put on and given a second wash, the low extension is attached to the building next to the entrance to the turntable attendant's lounge, the chimney and roof hatch have been installed, and the two gutter downpipes connected.

Finally, a few finishing touches followed. Because I didn't quite like the tiled roof yet, it got a second coat of soot with the anthracite-coloured wash. The chimney was a left-over part from a Kibri kit, bevelled on one side at the bottom. After the brick-coloured paint and the anthracite-coloured glaze, the chimney with the bevelled underside was glued to a piece of paper with plastic glue.

After the glue had dried, I used a scalpel to trim the paper so that a rectangle with a small even overhang remained under the chimney. I painted this protrusion and a narrow strip of the chimney with dark grey acrylic paint to imitate the roofing lead of the chimney roof seal.



After the work is done, the locomotive dispatcher can be seen performing his duties at his desk in the top floor office.

I attached the finished chimney to the roof with Noch grass glue. Then I made a rectangle of 0.3 mm thick polystyrene sheet with an edge length of about 3 x 3.5 mm, painted it dark grey, and glued it at a small distance next to the chimney, as a roof hatch for the chimney sweep.

The same material was then used to make a box-shaped, low extension with a sloping top; the three double flaps on the top are represented by paper rectangles. Painted in medium grey and with a light anthracite glaze, this extension was glued to the outside wall of the building next to the entrance to the turntable attendant's lounge in a prototypical way.

The missing rainwater pipes were supposed to be made of a 0.5 - 0.7 mm thick Evergreen plastic round rod, but since I only had 1 mm round rods at hand, I carefully heated one of them over a candle flame and pulled it out.

I then bent two pieces of about 0.6 mm diameter into the desired shape, painted them with medium grey acrylic paint, and attached them to the gutter and the lower edge of the building with plastic glue.

Setting up the control tower

At the installation site of the locomotive control tower, two 1.2 mm wide openings for the two connections were drilled through the layout plate and slightly widened at the upper edge. Somewhat stronger extension cables with an outer diameter of approx. 1 mm were soldered to both connection strands, the extensions were carefully pushed down through the two holes, and pulled until the soldering points got stuck in the holes at the top because of their larger diameter.

Related literature:

- Bernard und François Huguenin
- Bw Ottbergen: Ein typisches Bahnbetriebswerk der Dampflochzeit
- Horst-Werner Dumjahn Verlag, Mainz 1984
- Bernard Huguenin / Karl Fischer
- Ottbergen - Klassiker der Dampflochzeit
- Band 1: von den Anfängen bis 1960er
- Band 2: 1970er bis heute
- Eigenverlag Modellbundesbahn, Brakel 2011
- Andreas Hegger / Ulrich Marks-Fährmann / Klaus Restetzki
- Grundwissen Bahn
- Verlag Europa-Lehrmittel, 4. Auflage, Haan-Gruiten 2008
- Eisenbahn Magazin 2/1982
- Seite 52 ff., „Bahnhof Walkenried“
- Alba-Verlag, Düsseldorf 1982

Using a thin, narrow piece of cardboard as a flat brush substitute, I transferred Noch grass glue in places to the underside of the outer walls and then placed the building on the layout, aligned it and pressed it down lightly.

After the glue had dried, one of the two connecting cables was fed under the layout to a light-current-ground distributor. The light circuits of my layout are connected to their earth poles. The phase is routed via a distributor under the layout to the control panel. I have connected a 3.3 kΩ ballast resistor (R1) directly at the distributor to protect the LED (see circuit diagram on page 35).



The locomotive line construction has moved to its planned location on the layout. Now, the services of the steam locomotives based here are also planned in keeping with their status.

In the control panel, I can use a switch (1/0/1) to set two different brightness levels or switch the LED off. For this purpose, I connected the two output poles of the toggle switch with an additional resistor (R3; 8.2 kΩ). Depending on the position of the switch, the current is either conducted directly to the LEDs, or through this additional resistor, which significantly reduces the brightness of the LEDs, a desirable effect in night mode.

In the following part of this small series, I will describe the construction of my signal box, which has to replace a predecessor structure and should avoid previous mistakes.

All unmarked photos in this article: Jochen Brüggemann

Professional layout engineering:

- <https://www.architekturbedarf.de>
- <https://www.busch-modell.info>
- <https://www.faller.de>
- <https://www.noch.de>
- <https://www.preiserfiguren.de>
- <https://www.rotring.de>
- <https://viessmann-modell.com>
- <https://www.z-hightech.de>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Das Standardwerk zum ICE **Rasend schnell überholt**

Nach nur sechs Jahren galt es, ein Standardwerk über deutsche Hochgeschwindigkeitszüge umfassend zu aktualisieren. Das ist im Großen und Ganzen gut gelungen, hinterlässt aber auch Wünsche nach teilweise anderen Schwerpunkten. Und mit dem Erscheinen hat sich die Uhr auch schon wieder weitergedreht.

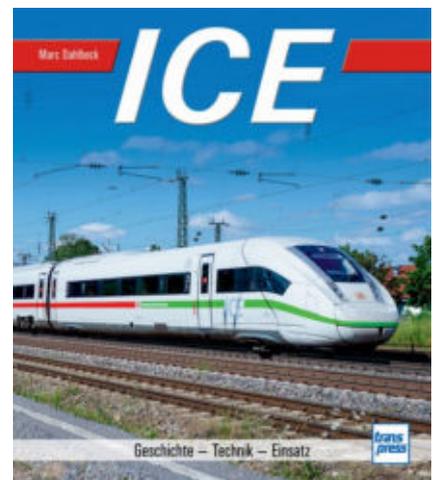
Marc Dahlbeck
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Geschichte - Technik - Einsatz

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In **Trainini**® 11/2016 hatten wir ein Buch gleichen Titels aus demselben Verlag besprochen: Diese Übereinstimmung ist kein Zufall, denn es handelt sich nicht um ein völlig neues Werk, sondern eine aktualisierte Neuauflage. Schon die 2016 erschienene Auflage hatten einen zehn Jahre älteren Vorgänger, der sich aber durch seinen Titel deutlich unterschied.

Dieses Mal ist es nach (nur) sechs Jahren nicht anders, denn politischer Wille ist, dass die Bahn ihr Fernverkehrsangebot mittelfristig kräftig ausbaut und auch Mittelzentren im Interesse einer „Verkehrswende“ wieder höherwertig an ihr Netz anbindet.

Das betrifft bei weitem nicht nur den Intercity, dessen Komfort auch zu diesem Zweck auf Nahverkehrsniveau abgesenkt wurde. Was er einst zu bieten hatte, wird mehr und mehr Alltag im Spitzenprodukt ICE, das mit den einst definierten Kernmerkmalen „Geschwindigkeit“ und „Komfort“ teilweise nur noch wenig gemein hat.

Nun denn, trotz sinkenden Sitzkomforts und beständig steigender Fahrpreise sieht die Deutsche Bahn ihren Fernverkehr auf Erfolgskurs. Zuletzt hat sie die bestellten, lokbespannten Talgo-Triebzüge „ECx“ für den grenzüberschreitenden Eurocity-Verkehr (Komfortstufe der IC) ebenfalls in den Rang von ICE erhoben. Das bedeutet nun eine Abkehr vom verbindlichen Konzept der Triebzüge.

Es bleibt also einiges in Bewegung und das Verwässern des ursprünglichen Konzepts scheint vollendet. Das Erfordernis, das Standardwerk für den ICE in überarbeiteter Form neu anzubieten und dem aktuellen Stand anzupassen, ist im Kontext dieses Abrisses zweifelsfrei gegeben – eine unveränderte Neuauflage wäre dem Bedürfnis der Leser deutlich zu kurz gekommen.

Mit dem überarbeiteten Werk hat sich nun erstmals der Autor geändert: Marc Dahlbeck, der auch schon mit Bildern im Vorgängerbuch präsent war, ist in diese Rolle gesprungen. Da er sich nicht nur als Fotograf, sondern auch als Fachautor einen Namen gemacht hat, ist er gewiss eine gute Wahl.

Und so hat er es geschafft, auf Basis der Manuskripte von Dieter Eikhoff die ICE-Geschichte (fast) bis zum heutigen Tag fortzuschreiben. Um Neues unterbringen zu können, musste besonders der geschichtliche Abriss vor der Erstpräsentation eingekürzt werden, trotzdem ist das jüngste Werk um 16 Seiten gewachsen.

Erfreulich ist aus Sicht derjenigen, die den Vorgängerband besitzen, dass auch die Fotos zu einem großen Teil ausgetauscht und so auch aktualisiert wurden. Andere sind nun in kleinerem Format Teil des Buches geblieben. An der Bildauswahl wie auch Abbildungsqualität lässt sich so auch nichts kritisieren.

Wunschlos glücklich macht uns die Lektüre dennoch nicht: Zum einen finden wir die internationale Erfolgsgeschichte der ICE-Züge zu kurz gekommen. Sie erscheint uns vor dem Hintergrund, dass TGV und ICE im internationalen Wettbewerb Konkurrenten sind und ihre Hersteller Alstom und Siemens zuletzt eine Fusion angestrebt hatten, sehr wichtig.

Gleichzeitig fragen wir uns, ob die Ausführungen zur Topographie, Sicherheitstechnik und Baufortschritt der Neubaustrecke (Stuttgart -) Wendlingen – Ulm tatsächlich ein so wichtiger Bestandteil der Überarbeitung sein müssen.

Anders als beim Shinkansen in Japan steht der Begriff ICE in Deutschland ja nur für die Züge und nicht die Strecken. ICE-Züge legen bis heute schließlich die meisten Kilometer auf (teilweise ausgebauten) Bestandstrassen und nicht den „Rennstrecken“ für 300 km/h zurück.

Unverändert vermissen wir kritische Worte zum bewusst niedriger gewählten Sitzkomfort und geringerer Geschwindigkeitszulassung in den Triebzügen der Baureihen 411 (ICE-T), 412 (ICE 4), 415 (ICE-T) und 605 (ICE-TD). Zwar werden die Probleme deutlich genannt, die besonders die Baureihe 605 immer wieder ausbremsten und unwirtschaftlich arbeiten ließen.

Kein Wort lesen wir aber beispielsweise über den ursprünglich geplanten Einsatzzweck aller vier Baureihen als Intercity-Nachfolger und die immer wieder kritisierten, zu kurzen Sitzflächen der ICE 4. Neutralität gebietet auch hier ein ehrliches Bild: Die DB hatte ihre Kunden immerhin mit Eigendarstellungen von mehr Beinfreiheit und größeren Sitzabständen an der Nase herumzuführen versucht.

Begrüßenswert ist das neue Kapitel 12, das sich ausführlich den erfolgten, laufenden und geplanten Lebensverlängerungen („Redesign“) der Züge widmet. Immerhin steht besonders die erste ICE-Generation schon über 30 Jahre im Plandienst, ist am Ende ihrer einst geplanten Nutzungsdauer angelangt und muss wohl doch noch fast zehn weitere Jahre Dienst tun.

Leider wirkt dieses Kapitel „mit der heißen Nadel gestrickt“: Gleich in der Einleitung verschreibt sich der Autor um satte zehn Jahre, was den Leser zu irritieren vermag. Im weiteren Textverlauf erscheinen dann ganze Satzteile doppelt. Dies wirkt, als sei dieser Titel unter großem Zeitdruck fertiggestellt worden.

In Summe handelt es sich um ein gelungenes Werk, bei dem leider der „ICE 3 neo“ (Baureihe 408; Siemens Velaro MS), der nun im Planverkehr steht, zu kurz kommt und die Talgo-Züge „ICE L“ (zuvor „ECx“) als offizielle Mitglieder der ICE-Familie noch völlig fehlen. So erscheint uns die nächste Überarbeitung, die erforderlich werden wird, schon greifbar nahe.

Publishing pages:
<https://www.motorbuch.de>

Wertvoll für jedermann

Der Modellbau-Ratgeber

Wer neu zum Modellbau stößt, der sucht zu allererst nach guten Quellen für wertvolle Ratschläge. Und wer schon lange dabei ist, der findet auch für sich stets viele Ansätze, sein Wissen zu verbreiten und Techniken zu verfeinern. Beide Bedarfe sind nahezu perfekt in einem Buch berücksichtigt worden, das wir Ihnen heute vorstellen möchten.

Mat Irvine
Modellbau leicht gemacht
Tipps und Tricks für Einsteiger und Fortgeschrittene

Verlag Delius Klasing
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Die vorliegende Lektüre ist vor drei Jahren im Vereinigten Königreich unter dem Titel „Model Builders' Manual“ erschienen. Die wörtliche Übersetzung aus dem Englischen trifft Inhalt und Umfang der Themenfülle besser als die griffig gewählte Bezeichnung für die deutsche Auflage.

Dem Verlag Delius Klasing ist es zu verdanken, dass dieses Buch über den Ärmelkanal auch zu uns gefunden hat. England darf, neben den Vereinigten Staaten, sicher als Mutterland des Plastikmodellbaus bezeichnet werden.

Bekannte Marken der Branche haben dort ihren Ursprung, es gibt eine treue und sichtbare Gemeinde mit diesem Hobby, die sich auch zu Ausstellungen und Wettbewerben trifft. In Summe ergibt das beste Voraussetzungen für eine Enzyklopädie über den Modellbau mit Polystyrol.

Modellbau ist ein fesselndes Hobby, wie der Verlag in seiner Buchinformation selbst festhält. Passionierte Bastler erschaffen beeindruckende Modelle aus Holz, Metall oder Kunststoff, heutzutage längst auch mit Technologien wie dem 3D-Druck. Ob es sich dabei um Modellflugzeuge, Eisenbahnen oder architektonische Meisterwerke handelt, erscheint zunächst nachrangig.

Das ist es auch, was diesen Titel für Modelleisenbahner interessant macht: Mit Polystyrol haben wir bei Eigenbauten und Gebäudebausätzen zu tun – auf den Modellbau mit diesem Material bezogen, ist es aber nur eine Facette von vielen. Hilfen, die diese Lektüren bietet, betrifft aber alle Enthusiasten gleichermaßen.

Und auch ergänzende Techniken wie additive Verfahren (3D-Druck), das Fotoätzen von Metallen oder auch besondere Teile aus dem Tiefziehverfahren durchziehen dieses Hobby in Gänze. Wenn sich der Autor dem Bauen, Optimieren und Lackieren von Bausätzen zuwendet sowie einen Blick in die Zukunft wirft, dann trifft er auch unseren Nerv.

Interessant ist aber auch der Blick auf die Geschichte des Modellbaus und ihrer Hersteller: Wie entstand er und konnte sich als Hobby etablieren? Wann und wie wurde der Modellbau als Markt erschlossen? Was wurde aus den Herstellern?

Dies sind nur drei von vielen Fragen, die im vorliegenden Buch beantwortet werden – und das auf eine packende Weise. Einmal zu lesen begonnen, mögen wir es gar nicht mehr weglegen. Auch als „alter Hase“ sind hier viele Dinge zu erfahren, die zuvor gewiss unbekannt waren.

Mat Irvine sorgt für Überblick in einem riesigen Angebot, lautet das Fazit des Verlags, dem wir uns gern anschließen. Er stellt die verschiedenen Genres und vor allem auch Maßstäbe vor, über deren Vielfalt heute sicher die meisten staunen, ohne zu ahnen, wie sie überhaupt zustande kamen.

Präsentiert werden auch wichtige Werkzeuge, erforderliche Hilfsmittel und sinnvolle Techniken, die deutlich über den reinen Zusammenbau hinausgehen. Berücksichtigt wird auch die Modellbahn, wenngleich auch nur am Rande.

Von Lackiertechniken und vom Wissen um das Herstellen und Aufbringen eigener Nassschiebebilder profitieren auch erfahrene Tüftler, die eigene Modelle aus dem Nichts erschaffen. Ein weiteres Thema ist der Dioramenbau, das als einziges für den Bereich der Modellbahn zu kurz und einfach gehalten ist. An dieser Stelle mangelt es uns aber eher nicht an Literatur.

Der uns zuvor nicht bekannte Autor hat sein Hobby zum Beruf gemacht. Das zu lesen, hat uns im Nachhinein nicht überrascht. Erfahrung und Können kommen in jedem einzelnen Kapitel nur allzu deutlich zum Ausdruck.

So eignet sich das Buch für Anfänger wie Erfahrene, für jung und alt, Flugzeug- und Schiffsmodellbauer wie auch Eisenbahner. Den Überblick fördern werden auch die Verweise auf wichtige Hersteller am Ende des Buches. Sie sind nach verschiedenen Kategorien getrennt zusammengestellt, bilden aber nur einen kleinen Querschnitt des gesamten Markts ab.

Bezogen auf die Modellbahn bedeutet das, wir finden hier keine alleinigen Hersteller von Rollmaterial, denn an der Stelle beliefern sie uns ja nicht mit Bausätzen. In diesem Bereich musste für Deutschland die Firma herhalten, deren Name als Synonym für das Modellhaus gilt: Faller. Marken wie Kibri und Vollmer, aber auch die kleineren Anbieter, fehlen.

Ein Manko sehen wir darin nicht, denn es handelt sich ja um ein Modellbau- und nicht ein Modellbahnbuch. So bleibt nur ein einziger, echter Schwachpunkt übrig, der auch gut zu verschmerzen ist und den Wert des Buches nicht in Frage stellt: Vor allem Aufnahmen, die in Ausstellungshallen gefertigt wurden, wirken erheblich zu dunkel und arm an Farben.

Ließe sich dies bei künftigen Auflagen beheben, würde sich der wahre Wert des Titels schon beim schnellen Durchblättern offenbaren. Sie würden zur Visitenkarte eines wahrhaft besonderen Werkes. Das möchten wir unterstreichen, indem wir es nun für die Neuerscheinungen des Jahres in der Kategorie Literatur nominieren.

Wenn Sie vom geballten Wissen des Autors profitieren möchten, dann sollten Sie nicht auf eine zweite Auflage warten. Egal, ob Sie gerade erst mit Ihrem ersten Bausatz beginnen oder schon einen Erfahrungsschatz vorzuweisen haben: Dieses Buch ist gewiss kein Fehlkauf und hilft, neue Maßstäbe zu setzen – vielleicht sogar im wahrsten Sinne des Wortes!

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Faszination Modellbau Friedrichshafen **An Exhibition with a new Name**

In the border triangle at Bodensee (Lake Constance), one of the few larger model-making exhibitions of this year, took place from 4 to 6 November 2022. Model railways once again played an important role and, with them, the 50th anniversary of Z gauge.

The beginning of November is trade fair time in Friedrichshafen – that's when friends of models from the railway, ship, aircraft, and car sections meet. In its 20th edition, it will be enriched by the large Live Steam meeting and, for the first time, under the illustrious name "Internationale Modellbahn-Ausstellung" (IMA) ("International Model Railway Exhibition").

This event was attended by a total of over 43,000 visitors. On the opening day, there was a lot of activity in the car parks shortly before the fair opened and visitors full of expectation crowded the fair foyer in anticipation of the opening.



This locomobile, whose siblings in smaller scales could be admired inside together with locomotives, created a Live Steam atmosphere even outside the exhibition hall.

For the first time, the trade fair company had consequently opted for electronic presale of admission tickets, which was also very well received. Over the morning, however, it became apparent at the few ticket counters on site, that not all possible visitors had been informed about the change in ticket purchase or were able to react due to a lack of personal access to the internet. Long queues and impatient, sometimes angry ones, were, unfortunately, the consequence.



The aisles are noticeably widening, small-series manufacturers are increasingly staying away: Let's hope that this will not be the case in the long term. A bargain at the fair was probably the exception rather than the rule this year, especially as there was a lack of well-known suppliers from the big players.

Model railways could once again present themselves to the public in two halls, and for the first time Hall A1 was available for this event. Large and bright, it offered ideal conditions.

High costs and the general economic situation make it more and more an incalculable risk to present one's business at this big exhibition.

The area around Bodensee (Lake Constance) offers short travel distances for model railway fans from Switzerland and Austria, but the number of eleven nations represented in Friedrichshafen was also international.

Well-known manufacturer names were represented on site, including Märklin, Faller and Noch, of course. Unfortunately, the quest for the EK publishing company, which was sorely missed with its outstanding range of literature, was unsuccessful.



One of us: presenter Hagen von Ortloff enjoys his retirement life and is always present as an exhibitor with heart and soul.

As was to be expected, Hagen von Ortloff and a few helpers and friends again showed old model railway treasures at his "home match". The joy of playing was the most important factor, and you could see it in his face. Pleasant conversations with the visitors and his obligatory autograph session were also part of it.

The play area for children, which was offered again, was very popular. Fathers and mothers played with their little ones, lingered and put right on the spot the very sense of our hobby in the foreground: joy and fun!



Always very popular were the soldering demonstrations by Torsten Schubert, who does not only want to impress visitors, but also to pass on his knowledge to them.

In terms of layout exhibits, the Z-Freunde International offered the real cream of the crop of this trade show and were very well attended throughout the entire day. Densely crowded, people looked and were amazed, but above all they talked shop.

Harald Hieber showed his Drachenstein layout again at a trade show after a few years. He was very pleased about the many technical discussions and the lively interest in his masterpiece. Rainer Tielke with his layout Moselbrücke Bullay was also constantly surrounded by amazed visitors.

And Uli Günther's Neuschwanstein Castle and Gerhard Maurer's Traumschleife were always very busy. Ingo Sindermann's dwarf layout was interactive. Young and old alike had a great time with him.

Jürgen Walther's Hirschsprung layout was very close to the real Höllental, which is the theme of his magnificent layout. Torsten Schubert's soldering demonstrations, as well as his showpieces, were in demand and the focus of interest.

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Whether young or old: Jürgen Walter's very successful "Hirschsprung" layout was fascinating for all visitors (photo above). In front of it, Gerhard Maurer's "Traumschleife" layout protrudes into the photo. The view into the Hirschsprung station (photo below) reveals why the layout is such an eye-catcher.

Oliver Kessler introduced his self-driving buses and trucks of the Spur-Z-Carsystem to the audience and was happy to answer their questions. Thomas Heß was also represented with his digital demonstration layout including new inner workings.



Oliver Kessler demonstrated his Z-Car system including the latest developments and answered visitors' questions. Many might have been concerned about the currently limited delivery capacity.

Roland Kimmich presented a few Railex treasures; Raffaele Picollo showed the Wuppertal suspension railroad, a golf course, and the Japanese small suitcase layout, which was already on display in Altenbeken. Also, "Dead End City" and "Nugget Gulch" by Michael Bernhard could not be missing from this show.

So, there was a lot going on at the Zetties, high quality layouts demonstrated to the visitors that with Z gauge, even the highlights of a big show are on display by now.

By the way, the booth of the Z-Freunde International was the central meeting point for many familiar faces. Lütke Modellbau and Artitec were also represented with their own booths and their Z assortment, where the excellent models of our nominal scale found brisk sales.

Our scale was also represented by Märklin with a display layout and a showcase dedicated to the 50th anniversary. The workshop team gave some well-attended presentations on digital technology and other topics, and the product managers were on site and always approachable.

continues on page 50



Thomas Heß had redesigned the scenery of his digital demonstration layout (photo above). Märklin's new Z gauge trade show layout (photo below) was also well received by the visitors. Next to it, the manufacturer presented a cross-section of 50 years of Mini Club.

At the Insider Club, Silvia Römpp herself took care of distributing small presents to the members. Museum cars were also available for purchase at the sales booth. Faller had brought a nice diorama with its current Z gauge models, otherwise the representation of our scale, also; and especially with the dealers, was, unfortunately, rather limited.



The larger scales also had some nice exhibits to offer in Friedrichshafen. "West Side Lumber Co.," for example, was a logging railroad in garden railroad format.

Among the show layouts of the large gauges, especially Kooperation Holz (H0 scale) and the West Side Lumber Co., Model Railway Group (IIm gauge) could inspire. The live steam meeting was impossible to miss thanks to its smell and the "smoke signals". On the round trips, the seats were always much sought after, by adults, as well as, the children.

There were also real eye-catchers in the other halls with model aeroplanes and model ships, demonstrations with model trucks and excavators, as well as, snow groomers. The steam engines, which made technology directly come alive and visible, also gave a lot of pleasure.

To sum up this trade exhibition: very well organised, very well attended, a trip that was worthwhile for all model builders, because in Friedrichshafen it is always easy to look over the fence into other hobby areas.

.....
Exhibitor selection with Z gauge relevance:

.....
<https://artitec.nl>
<https://www.busch-model.info>
<https://shopware.donau-elektronik.de>

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<https://www.faller.de>
<https://verlagshaus24.de/geramond>
<https://www.halling.at>

.....
<https://www.luetke-modellbahn.de/>
<https://www.maerklin.de>
<http://mzero.de>

.....
<https://microrama.eu>
<https://www.noch.de>
<http://www.peter-post-werkzeuge.de>

.....
<https://www.saller-modelle.de>
<https://viessmann-modell.com>

.....
<http://z-car-system.de>
<https://www.z-freunde-international.de>
<https://z-stammtisch-bayern.de>
.....

Our review of the year 2022

The End of a special Year

2022 was not just any year for the Z gauge: it celebrated its 50th birthday. This was to form a common thread for a whole twelve months. And, despite only a few exhibitions and trade fairs, we managed to draw attention to our anniversary together, broadly and proudly.

It is a cherished tradition for us to pause in December and look back at what has moved the Z gauge community in the past calendar year. This can of course be seen in the issues of our magazine, but sometimes even this look might fall short.

2022 should not be an exception, because, after all, a special year is coming to an end for what was once the smallest model railway in the world. Certainly, we would have liked to have had more exhibition and trade fair presence in order to draw more attention to our scale and to promote it.

But at least on the part of the editorial team, we are pleased to note how well our idea of a common anniversary logo for all groups, associations and also manufacturers has been received. It was frequently seen on product packaging, websites, and also in films, thus creating a unifying image.



The year 2022 also began with a review and awards. Among others, the "Obermurrhärle" layout by Karl Sinn shown here was considered.

Let's now go through the course of the year again: In January, we always announce which products and services we will award as the new release of the year. For the first time in three years, Märklin did not win this award, which can certainly be described as extraordinary.

However, it should not be understood as a decision against the market leader, but rather as a clear indication of a high-performance small series manufacturer. Even if some classic suppliers have said goodbye in recent years, currently seem to have disappeared or are planning to retire soon, this proves a very lively and, therefore, sustainable scene.

In the same month, our annual focus on 2021 came to a worthy conclusion. We portrayed the Z gauge studio, whose operator Karl Sinn was awarded a prize for his layout "Obermurrhärle" in the new publications of the year.

How could it be otherwise, February was dominated by the spring new products. When a milestone anniversary is coming up, everyone looks particularly closely at what the manufacturers are trying to sell them.



Märklin already showed a sample of the Re 6/6 in Altenbeken, which was apparently made from series parts. At the turn of the year 2022/23, this locomotive is nevertheless still a long time coming.

At Märklin, there were three new locomotive models, one for each type of traction; the DR's class 01⁵ express steam locomotive, the SBB's Re 6/6 electric locomotive, and the DB's V 320 diesel locomotive. At the time of going to press, however, we also have to note that none of these new products has yet been delivered. At least with the V 320, according to Märklin's prognosis from the latest delivery date list, it should still be possible shortly after Christmas.

However, the fact that the announced anniversary starter pack has also fallen by the wayside is more indicative of tight capacities and problems that require production runs to be adjusted and postponed during the year.

The models contained in a historically reminiscent cardboard box recreate the diesel locomotive class 216 from the launch year 1972 together with the pop-coloured express train wagons at the high technical level of today.

So, they should clearly show how much has moved in 50 years. Now it will definitely be a retrospective. Let's take it with a sense of humour and interpret it as a Deutsche Bahn start-up package: The train is coming later – disruption in the operational process.

Faller has been a loyal Z gauge accessory supplier for decades. So, the Zetties didn't go empty-handed there, but sometimes had to wait until autumn. All the greater was the joy that on the occasion of the round anniversary, a PS kit from the time of the market entry will be issued in new colours, quasi as a retro kit, and there will also be a special hard cardboard kit with the Dreiseithof – history and future perfectly united.

One of the big surprises of the year, which we were later able to honour with the testing of locomotive and carriage models, was the market entry of Azar Models from France. Apparently, the programme went so well that they had to switch very quickly from 3D printing to plastic injection moulding for a covered carriage.



One of the biggest surprises of the year came from the French small-series manufacturer Azar Models with BB 67400 and an SNCF boxcar that can also be used internationally.

That speaks for significantly higher requirements than expected. We remain curious as to how things will continue and whether the manufacturer will perhaps also open up for DB models in cross-border traffic. What comes to us from France on German track has already found its way into the range and enriches the offer in a commendable way.

Modellbau Laffont has announced and meanwhile delivered a real offensive of self-built kits. Selected models from their new product range will also appear in our news in the near future

NoBa-Modelle celebrated its 5th birthday, and not only we in the editorial team will have wondered if so many years have really passed, again. As was expected, this duo also stepped on the accelerator hard and produced exciting models. As a highlight for the year 2022 we have the Ardel steam crane 57 to., which not only looks impressive, but also offers many possibilities of use.

Since 24 February 2022 there has been war in Europe, again, because on that day the Russian army invaded its neighbour Ukraine. It was bitter that among the many people who were and are affected by this attack, there is at least one family that is well known in our community.



Norbert and Barbara Heller certainly didn't just give themselves a present with the Ardelt crane train on their own five-year company anniversary. Photo: NoBa-Modelle.

Trainini®, the 1zu220-Shop and the Z-Freunde International e.V. felt that something had to be done quickly, coordinated their efforts, divided the tasks among themselves and launched an unprecedented relief campaign. Many Zetties, we cannot count them exactly, participated in many different ways.

They offered and provided fuel, accommodation, meals, money, and other donations, in kind. They made it possible for Oleksiy Mark, a small-scale manufacturer and now **Trainini®** translator, and his children to escape from rocket and bomb-ravaged Kiev to safe Germany.

The refugees found accommodation in the editor-in-chief's house for a fortnight before they were able to move into their own flat in record time. In the meantime, the children go to school in Germany, have improved their language skills, made friends, and their father is also working on a new life in normality.

In March we started our 50th anniversary report series with a look through the Märklin programme. In it, we also described how the manufacturer was driven by the small-series manufacturers to further develop its programme technically, because at the beginning it was very, very slow. Also worth mentioning is the 111-year company anniversary at Noch in the Allgäu, which we had also referenced.



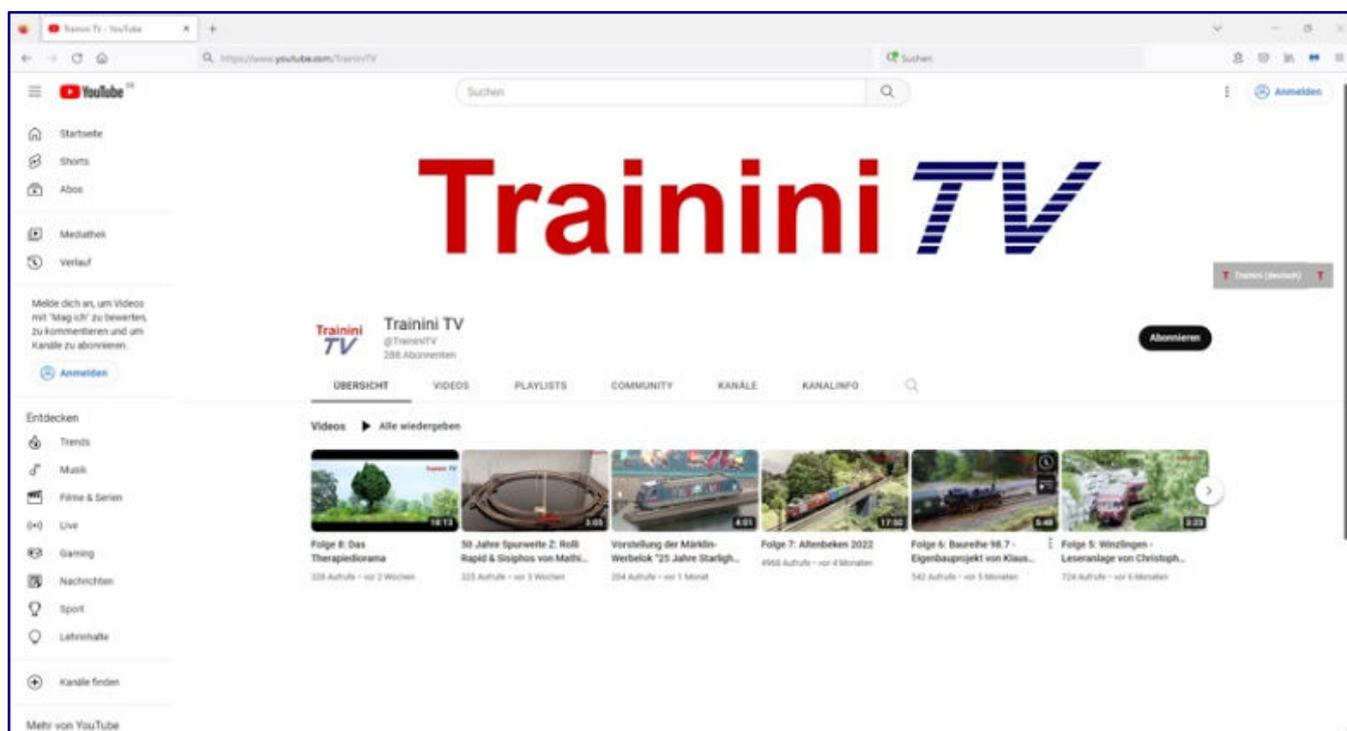
The RAM TEE of the SBB was a double topic in April: We presented it as a digital sound model by NoBa-Modelle, and, at the same time, also a home-made model by our reader Yuji Kuwabara.

April also offered something special, and certainly not only from our point of view. With the SBB's RAM TEE multiple unit, we presented a model that many Zetties would like to see, and in a digital DCC sound version to boot, which is unlikely to have much of a chance at Märklin.

We framed this article with models built by a Japanese reader many years ago, but which were now particularly topical again. Even though we did not include this article in our annual focus, the report was proof of how international 1:220 scale really is. And, that is a special feature of which we are very proud.

We are also proud of our YouTube channel **Trainini TV**, which was launched on 23 April 2022. After just one month, it already had enough subscribers to give it a catchy address with <https://www.youtube.com/TraininiTV>.

Although we did not have enough free time to produce and publish all the films we had planned for the year as a whole, the response remains good. Most recently, we were able to add another focus with tips and explanations on handicrafts, which is gratefully received. A little later we welcomed Ralf Junius as a new helper in the team.



We are also proud of our film channel, which was launched on the YouTube platform on 23 April 2022. As of the publication of this issue, eight episodes of **Trainini TV** have already been posted there, in addition to old film contributions, and one from our readers.

Ralf supports us at **Trainini TV** with his knowledge and skills. In the video conferences of the editorial team and translator group, he then also introduced himself and quickly settled in. We hope that he will enjoy this job for a long time, because we appreciate his help, which complements us well.

In the merry month of May, most of the lines in our issue belonged to the new Vectron from Märklin as well as to its great prototype. This modern electric locomotive is certainly a key model to ensure that our scale remains viable for the future and can also appeal to those interested in contemporary railways. We have completed the magazine with an article on car models, among which, EtchIT-Modellbau, in particular, stands out.

The transition to June is made by the International Z Gauge Weekend in Altenbeken, because this time it took place in mid-May and our report appeared in June. The number of exhibitors and visitors was lower than at previous editions, but this was to be expected, in view of the unclear situation regarding the Corona pandemic.



Altenbeken was also the venue for the symbolic handover of the cheque from the fundraising campaign launched by Trainini®, the 1zu220-Shop and Z-Freunde International e.V. for Oleksiy Mark and his family from Kiev. Photo: Stephan Fuchs

In terms of quality, it was not behind, and Märklin used the extra space to celebrate its anniversary, and, also, to present a new exhibition layout. Among the many exhibitors, it is difficult to single out one, in particular – the only thing left to do is to refer to the monthly issue. However, the creative ideas of Trafofuchs with special figures made a lasting impression on us.



The highlight of the year for the homebuilt sector was certainly the 987 series by Klaus Büttner with the assistance of Wilfried Pflugbeil.

Another highlight of the early summer was the impressive egg-building project of the class 987 in the form of the “Zuckersusi”, tackled and led to a record-breaking result by Klaus Büttner from Chemnitz. It served as a showcase project for the creative part of our community as a significant part of “50 years of Z gauge”.

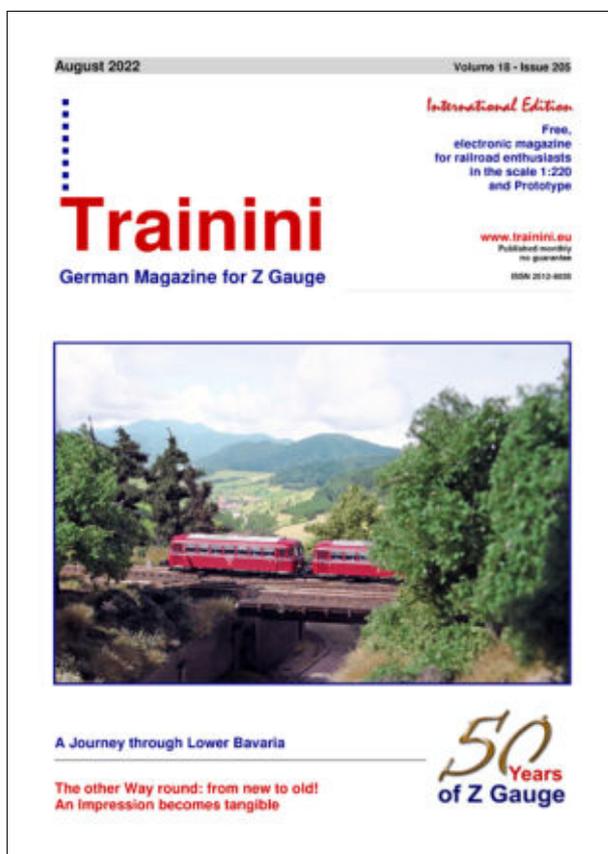
The 15th of June 2022 also remained in the memory of many Zetties, some with joy, others with frustration: At the suggestion of our magazine, the 1zu220-Shop decided to offer two Freudenreich special models as a souvenir of the anniversary.



It was difficult to calculate the appropriate edition for the special model "50 Years of Z Gauge" of the parcel post car Post 2ss-t/13 with old post horn and Epoch IV inscriptions—the entire edition was sold out after only 90 seconds.

The project management was entrusted to us; the number of pieces was agreed between the buyer and the supplier. It was tight, but the next batch size would not have sold in the same way according to a uniform assessment. The result was that the first model was sold out exactly 90 seconds after it became available.

Some customers were not even able to reach the site within this time window. This obviously stuck in the mind, because considerably fewer people tried the second model in November. In this case, it took longer, but not even 24 hours, to sell out completely.



In the second half of the year in particular, however, such successes tend to be an exception. Soaring inflation and, above all, the crushing prospect of exploding energy costs changed the focus in Germany; model railroaders are no exception. Some dealers indicated a drop in sales of up to 75% – hopefully, only a temporary and short-lived photo.

Many readers found what we presented in July to be an interesting thought: Is the "crocodile" really Märklin's heraldic animal? Or is it secretly not the "beautiful Württemberger"? After all, the people of Göppingen have known how to celebrate many occasions with this steam locomotive that had a close connection to their homeland. An outline of the model's history brought clarity.

In August, we had to cope with special conditions. Due to absences, the editorial team shrank to just one person. For five weeks, Dirk Kuhlmann had to steer the fortunes of **Trainini®** alone.

While in the first phase of the creation of our August issue he was still able to rely on supplies from our editor-in-chief, he was then left to his own devices two weeks

before publication. It is good when a functioning group stands together. In this phase, Stephan Bauer was particularly important as a technical supporter.

Our authors were an energetic help, and their hard work paid off all the more during this phase. Heribert Tönnies contributed significantly to the content of the issue, Hans Helbach agreed to write a review, especially since about one fifth of the title reviewed was penned by editor-in-chief Holger Späing, and he was consequently ruled out for this role.

Then, in late summer, it became apparent what a healing effect model railroading can have on some people. "The model railway as therapy" was the headline for a new diorama project, which described the work of occupational therapy in three parts – from impressions on walks, via idea and plan, to the realisation of the landscape.



The construction of the therapy diorama "Am Haarstrang" was, finally, the happy end of a time of suffering. Along the way, we could hopefully also show ways in which the model railway can point the way back to a balanced everyday life.

There is no question that the long period of deprivation and lack of contact has left its mark on many of our readers. We hope that model railways can contribute to restoring balance and a healthy state of mind. If we have succeeded in doing so, this continuation of the series and our contribution to **Trainini TV** (episode 8) will have made a difference.

In September and October, our theme for the year made us think outside the box: We explained how Z gauge is positioned today in the important foreign markets of North America and Japan and also included a little history of the important manufacturers there.

With the V 188 001 a/b we presented another particularly important model. It is special because it is heavy and powerful, and, therefore, a sought-after locomotive for long goods trains. Eight axles, all driven,

Trainini® Photo Calendar 2023

German Magazine for Z Gauge



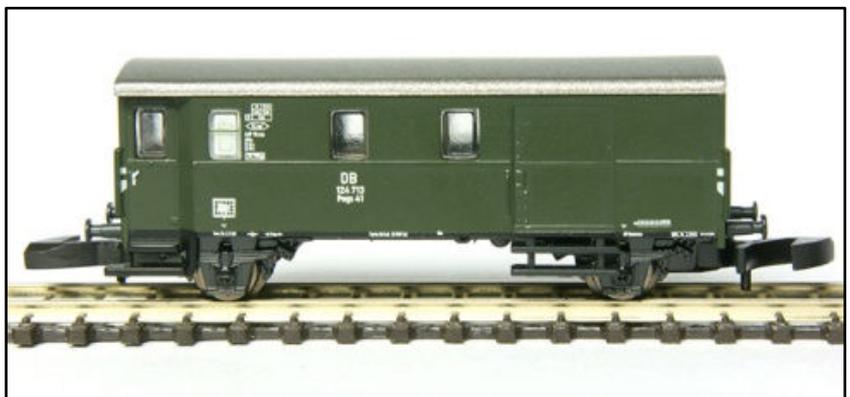
and thanks to the cast housing sufficient weight, proved to be a target-oriented concept, which we gave a lot of space to together with a prototype report.

Equally special is the new Pwgs 41 goods train baggage car, which was reviewed and presented in the following issue. After all, this is only the second wagon of this type in the Märklin programme and its predecessor had to endure a full 50 years on its own. It has long since been considered obsolete and we would just like to note: Once almost every goods train carried such a wagon.

The jubilee was also heading for a climax in our reports: With the accessory and small series manufacturers, we have examined the large mass of market participants.

There were too many to list them all by name and without exception, but we have certainly been able to show their importance.

And we were also able to share the impressions and motivations of our readers with everyone. We let the ten winners of our call, whose lot was drawn by "lady luck" Kristin Späing, have their say. We would have liked to include more voices in the



With the Pwgs 41 we were able to present another new goods train luggage car from Märklin.

magazine, but, in the end, the only fair choice seemed to be between all of them or those who were lucky enough to win.

This brings us to the present day: some surprises could certainly be waiting for us under the Christmas tree and maybe the few models that are expected soon will make it to the dealers' shelves. One such surprise that not everyone has discovered yet is the new **Trainini Fotokalender 2023**, which we made available on our pages on 8 December 2022.

The Model Railway Day was definitely a success for our nominal size, as can also be read in the reports of this issue. Committed Zetties met, exchanged ideas and above all presented our hobby to the public.



The announcement of the Insider Club models for 2023 marks the end of the anniversary year, and at the same time provides a first preview of the coming twelve months. Certainly, they will be no less exciting than the big year of our little gauge. Illustration (volume representation of the ESA 150): Märklin

Our common logo was once again clearly visible and reminded us of how alive we are. And so we hope that in the new year we will all, perhaps, be allowed to “speak much louder,” again. Märklin itself has already put us in the mood for this: The ETA 150 together with the ESA 150 driving trailer announced at the end of November has been a big wish. Let's hope that we don't have to wait until 2024 for it.



Directory of our licensed **Trainini Archive**:
<https://www.trainini.de/spur-z-welt/trainini-archive>

Our film channel - **Trainini TV**:
<https://www.youtube.com/TraininiTV>

Season's greetings

Dear readers,

Editors, translators and all other volunteers of our magazine wish you a merry and blessed Christmas.

And because it is the important things that we need most for a happy and contented life, we especially wish you all good health for a good year 2023.



We would like to take this opportunity to thank you for your ideas and loyalty to our magazine.

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

The V 188 from Märklin inspires:

The report in **Trainini®** 10/22 about the V 188 and the beautiful colour photos of the original had inspired me very much, so that my model now had to follow suit. Besides the correct size buffers and white lower lamps, the undercarriage and the car body were treated with an airbrush.

All glued-in windows were protected with masking paint. Dust, grey and sepia paint were used, as well as Weinert black RAL 9005. Afterwards, a clear coat was applied with Bergswerk, and as the icing on the cake, the exhaust openings were applied in black, as a round dot.



Our reader's V 188 001 a/b can now be seen all too clearly in its model form. Photo: Dirk Rohwerder

After that, everything was reassembled. Due to the good driving characteristics, it is really fun to shunt with this locomotive and to drive over the turntable. I would like to express my thanks again to Märklin for this great insider model.

Dirk Rohwerder, Sprockhövel

Thanks to Trainini®:

I've been collecting Z gauge for 4 or 5 years now and have only just stumbled across your site.

I am very enthusiastic about your great, professional, highly ambitious, and committed work! And I have only just skimmed a few issues! And the whole thing on a voluntary basis. Wow.

David Barry, München

Review of Model Railway Day:

Around 2 December 2022, which, at the suggestion of Hagen von Ortloff, has been celebrated for several years as International Model Railway Day, many events and exhibitions were again held to bring people closer to our hobby.

The manufacturers themselves are also taking part, for example, Märklin and Faller have incorporated the holiday in their company museum in different ways or provided offers to customers. Faller is also involved in a model railway exhibition in a furniture store for a period beyond the day.



In Ralf Junius' arrangement of regulars' table boards on Sunday there was also room for our diorama "Am Haarstrang", which illustrated the current state of landscaping.

We had a look around at two events: we were thrilled by the selection that Markus Schiavo, prominent in the nominal scale N, was again able to assemble at the Museum of German Inland Navigation in Duisburg. Despite three cancellations due to illness, 27 exhibitors set a new record.

Of course, in addition to model railways, model shipbuilding was also a topic there, partly also in a combined form. Layouts and presentations covered common gauges from 1 to Z. For our scale, three participants held the flag high.

Ralf Junius, long since also part of the **Trainini**®-team, presented a model railway Advent wreath and a layout made of members' layouts. In this way, he impressively showed the visitors the meaning of the sentence: "There's room even in the smallest hut!" On Sunday, he received additional support from our therapy diorama "Am Haarstrang," which thus celebrated its exhibition premiere.



Here you can see a city scene of the layout "Stellwerk Marl-Sinsen", which André Kammels presented on the occasion of the Model Railway Day.

Volker Bastek also provided an impressive backdrop for our photos; the object of desire was his home layout "Altenmühl", which looked strangely familiar to us, and yet offered many motifs and much variety.

André Kammels presented a small exhibition layout with city motifs of the Ruhr area, a theme that is rarely realised. There was also always a lot of train activity around his "Stellwerk Marl-Sinsen".

Visitors with local knowledge proved how authentically the drivable diorama had been realised, talking about the former terminus of a tram line just beyond the edge of the diorama or about nearby churches.

For the second time this year, the Verein Modelleisenbahner Soest 03 e.V. (MES 03) also opened its doors to visitors. On the operating day, the guests could marvel at many trains on layouts in gauges H0, N, and Z, as well as buy used material from the club's collection. The physical well-being was also well catered for here.

The large Z scale modular layout with landscape, town, and industrial motifs invited visitors to follow the rolling stock on its long journey. Unexpectedly, for many non-experienced visitors, digital operation prevailed here.



The Intercity "Max Liebermann" passes through the small Osterburken railway station on the MES 03 club layout during the driving day for Model Railway Day 2022.

Some of the models, such as the NOHAB from Märklin, were also an acoustic delight, because among the Velmo decoders installed there were also those with operating sounds. Equally pleasing for us were the shining children's eyes that we met at every corner of the individual layouts.

The **Trainini Photo Calendar 2023** is ready:

The **Trainini Photo Calendar 2023** has been available for download on our website since 8 December 2022. Up to DIN A3 (landscape) format, it can be printed out on paper by our readers and hung on the wall.

13 beautiful motifs, including the cover photo, will hopefully sweeten the months of the coming year and offer the usual variety of countries, railway administrations, seasons, traction types, and eras.



Rhine-Neckar Club Car 2022:

The Z-Stammtisch Rhein Neckar remains true to its line and offers a Club car again this year. It is also explicitly dedicated to the 50th anniversary of Z gauge, which is recorded with the community logo on the box insert. This anniversary coincides with the 20th anniversary of this group.

A covered freight wagon Gmrehs 30, a former "Oppeln", which is permanently rented (item no. 49.334.221), was chosen. Labelled for Era III, it thus bears the white-green colour and inscriptions of the mineral well "Staatl. Fachingen". The model was again produced by FR Freudenreich Feinwerktechnik.

The mineral water supplier has its headquarters directly on the Lahn Valley Railway, which has been in operation since 1863. That is why it used the railway early on to transport its bottled water. Photographs from the fifties and sixties served as a basis for the club car.



The Rhine-Neckar 2022 club' car is dedicated to the mineral water brand Staatlich Fachingen. Photo: Stammtisch Rhein-Neckar

The retail price is EUR 98.00, and this model can be ordered via info@kurpfalz220.de. At the time of going to press, 15 of the 59 manufactured and numbered models were still available. The price includes an aluminium bottle opener from the licensing company.

In the spring of 2022, the Stammtischwagen (club wagon) 2021 pack was also presented in Altenbeken, consisting of two carriages, and was also produced by FR Freudenreich Feinwerktechnik.

Included are a UIC type 2 open freight wagon (corresponds to the German Omm 55) of the SNCB and a "Typo F" pointed roof wagon of the FS.

The first of the 250 wagons once ordered by the SNCB were delivered in the late fifties still in green livery, just like the model. Subsequently, the carloads for these models were also completed, so that delivery can start. This is a removable hoop-rolling barrel load from Rainer-Tielke-Modellbau, which was coloured in different ways by the club.



This duo (photo left) forms the regulars' wagon pack 2021. The rolling hoop barrel loading inserts, which were missing until now, are now also available and have been individually, i.e., differently coloured (photo right). Photos: Stammtisch Rhein-Neckar

The Italian wagon in the typical covered design has the oxide-red paint of the original and a window-grey roof. Special features of the wagon are the imprinted paper notes of the consignor "Tavoni" and the freight notes glued to the black chalk fields, which were not part of the standard editions.

In this case, of the 40 packs produced, seven packs are still available at the time of going to press. They will be sold at EUR 189.00 each. We have already mentioned the order contact address above.

Rather few Märklin deliveries:

The Christmas business in Z scale will probably fail to a large extent, and this will not only be due to buying restraint. Märklin has only been able to put three new products on the market since the publication of the November issue.

The models announced for this month have not yet been released. Available, however, is the trio of three-axle conversion wagons (art. no. 87062), which are permanently close-coupled. Therefore, a rerailing aid is included in this package. The wagons were once on the Geislinger Steige in this consist.

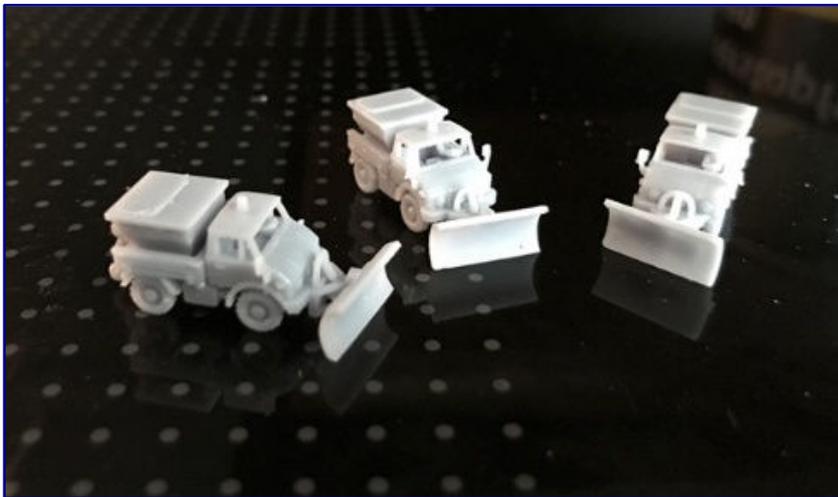


The wash edge on the two left wagons has always been visible on the official product illustration as well. Customers can now see for themselves how they look in natura, because the three-part wagon pack (item no. 87062) for the Geislinger Steige has been delivered. Photo: Märklin

The chrome oxide green wagons with Era IV inscriptions also have a special feature: The so-called wash edge on the roof is reproduced on two examples. This means that the outer sides, which are reached by the brushes, are clearly lighter than the not cleaned middle strip.

Also arrived is the five-piece wagon set "Timber transport of the PKP" (86683). It contains four Eanos 415 W high side wagons, for the first time with attached brake reversing levers and without protection bracket, and a Res 424 Z stake wagon of the Polish logistics company PKP Cargo. They are used to transport long and stamped timber. All models have close coupling couplers and are reproduced in the conditions of Era VI.

Shortly before the editorial deadline, the first SBB electric locomotives Re 420 in the "175 Years of Swiss Railways" (88596) version arrived at the dealers. The models with bell-shaped armature motor and LED light change have black nickel-plated single-arm pantographs from the new design and have been digitally printed.



The Unimog U 406 with snow plough (art. no. 6240R) is ideally suited to the onset of winter. Photo: NoBa-Modelle

News from the box train operator:

As new products, all Ks signal types can now also be ordered from this supplier with beacons, and, finally, also as a variant with an articulated mast or straight mast. Kastenbahner (<http://www.kastenbahner.com>) now also shows many model railway videos on Instagram.

Winter news at NoBa-Modelle:

NoBa-Modelle (<https://www.noba-modelle.de>) is now offering a truly wintery model: the Unimog U 406 is not only available as a basic model (art. no. 6241R), but also with a snow plough (6240R).

With the Mercedes-Benz Arocs 8x4 tractor unit (6018R) and the low-bed semitrailer (6071R), heavy goods vehicles also get their money's worth. The M22 multi-car (6239R), whose history dates back to the GDR, provides good service in a smaller format.



The VT 18.16 was the flagship train of the Deutsche Reichsbahn and can now also stroll along the Z gauge parade routes in the future. Photo: NoBa-Modelle

The diesel multiple unit VT 18.16, which the DR once had built as a counterpart to the West German VT 115 for high-quality passenger service, has the same origin. This four-part unit appears as a new blank (5230R) as well as a finished model (5230RF).

New military vehicles at AZL:

Milwaukee Road's EMD E7 (art. no. 64608-1 / -2) and D&RGW's ALCO PA1 and PB1 (64404-1 and -2 / 64404-1_SET and -2_SET) are currently awaiting delivery. In addition, there are three blue MAXI-IV sets of the Florida East Coast (906555-1 to -3) with various container loads.



D&RGW's ALCO PA1 and PB1 (part no. 64404-1_SET; photo above left), Milwaukee Road's EMD E7 (64608-1; photo above right), Florida East Coast's MAXI-IV sets (906555-; photo below left) and TTX's 89-foot flat cars with Military Load (AZL-911010-6M10G; photo below right) make up AZL's December new products. Photos: AZL / Ztrack

New in form are the heavy military tractors M1070 including trailers M1000 supplied by Z-Panzer, which are offered in sand yellow (AZLM1070_M1000S), and in olive green (AZLM1070_M1000G).

They are also loaded in these colours on 89-foot TTX flat wagons (911010-5M10S / 911010-6M10G / 911020-2M10S / 911020-3M10G). Manufacturer photos of the current deliveries can be found at <https://www.americanzline.com>.

Christmas at Trafofuchs:

In addition to several other figures that have recently found their way onto the Trafofuchs pages (<http://www.trafofuchs.de>), there is now also a new product in the range that fits in perfectly with the season: the horse-drawn sleigh. In it, a family of four is on its way to Christmas mass to celebrate one of the highest festivals.



It's getting Christmassy with the Trafofuchs horse-drawn sleigh, as the family is on its way to Christmas mass. Photo: Trafofuchs

In our illustration they are dressed in contemporary clothing, but can also be ordered with clothing from earlier times on request. This new product is brand-new and can no longer reach interested parties at Christmas, when this issue is published. It may therefore also be understood as a new product for 2023, and be planned for the coming festive season.

Intermodellbau Winning model:

In August a photo competition was launched, which we had also mentioned. We were looking for the most popular models in four different model-making categories, which will become the "face" of Intermodellbau in future advertising campaigns.

In the category "On the rails" Sebastian Mühlbach won first place with his patinated 64 295. His prize is a large trade fair package worth a total of around EUR 1,000.



Sebastian Mühlbach's 64 295 won the Intermodellbau competition in the category "On the rails." Photo: Messe Dortmund

Micro-Trains follows suit:

Micro-Trains is currently delivering further models of the 70-foot container wagons of the type “Husky-Stack.” Again, these are red and blue representatives, this time for the Burlington Northern (Item No. 540 00 023 / -024), Southern Pacific (-043 / -044), BNSF (-061 / -062), CSX (-113 / -114), and ATSF (-153 / -154).

In the special wagon series “Railroad Magazine”, wagon number 10 (502 00 649) is now on its way to collectors with a historic cover motif. The manufacturer (<https://www.micro-trains.com>) shows illustrations on its webpages.

Advance ticket sales for the Spielwarenmesse:

Spielwarenmesse (International Toy Fair) and well-known companies from the model railway and model building segment are giving interested end consumers the opportunity to experience the new products in person in Hall 7A on 4 and 5 February 2023. We had already reported on this announcement.



End consumers will only be able to access Hall 7A on 4 and 5 February 2023 via the East Entrance. Tickets can only be purchased in advance, electronically. Photo: Spielwarenmesse

Since 5 December 2022, tickets can be purchased electronically in advance at a price of EUR 6.00. No tickets will be sold on site! Access to Hall 7A on the weekend is also free for children from the age of six, and up to the age of 17. Products cannot be purchased there.

During this special opening, access for end consumers will be exclusively via the East Entrance. A shuttle bus will take visitors from the Mitte entrance and the nearby underground stop „Messe“ (“Trade Fair”) directly to the exhibition.

New products and news from Herpa:

As usual, we list here the new Herpa Wings products in 1:200 scale that were or are to be found at European airports and do not exceed an overall length of 30 cm. For the months of January and February 2023 these are:

KLM Exel ATR-42-300 (Art.-Nr. 571654),
LOT Polish Airlines Ilyushin IL-18 (572446),
TAP Air Portugal Douglas DC-4 (572453),
Swissair Douglas DC-4 (572491), and
UTair Ukraine ATR-42-300 (572651).

Based on non-civilian models, a Sécurité Civile Douglas DC-6 (572484) and for the French Air Force a Transall C-160 R "Last Transall Flight" (572569) appear. New models from the "Snapfit" series have not been announced.

In the meantime, the last issue of the magazine "WingsWorld" has been published, and at the same time the previous Herpa customer clubs will be discontinued. This had already been announced four months ago, but Herpa has still not named the announced replacement concepts and formats.

Neue Wagen von WDW Full Throttle:

After the last editorial deadline, two new car duos and a collector's pack followed from this small-series supplier. The two double wagon packs are dedicated to the Santa Fe and contain two reconstructed 34-foot refrigerator wagons with wooden superstructures (both art. no. FT-9029-1), which differ in the service numbers.

The collector's pack "Appalachian Coalers" (FT-COL62) contains two black painted, open freight cars with P&LE and Interstate markings, which are delivered with coal load inserts. The models are shown on the manufacturer's website (<http://www.wdwfullthrottle.com>).

Museum car 2023 presented:

After a three-year break, the time had finally come again: the invited guests met again at the Märklinium in Göppingen for the Museum Car Vernissage. Many familiar faces had come to Göppingen, and they obviously enjoyed the opportunity for lively conversation that evening.



The invitation of the Märklin management to the vernissage of the Museum Car 2023 was well received.

The partner for the Museum Car 2023 is the Stihl company from Waiblingen, world market leader in the field of power saws. In his laudatory speech, Märklin Managing Director Wolfrad Bächle provided a detailed excursus into the company's history.

But first he reviewed the recent past at Märklin and also praised the Z gauge meeting in Altenbeken, where the Zetties celebrated their gauge in the anniversary year.



90-year-old Hans-Peter Stihl (3rd from left) from the owner's family was given a special welcome by Märklin Managing Director Wolfrad Bächle (4th from left), as a loyal customer and long-standing Insider Club member.

Bächle was able to welcome 90-year-old Hans-Peter Stihl and the management of the museum car sponsor as a special guest. He was pleased to point out that Hans-Peter Stihl's attachment to Märklin is reflected in the fact that he has been an Insider Club member since the beginning.

As a museum wagon for the H0 scale there is the sliding roof/sliding wall wagon of the type Tbes-t-66, which is accompanied by an Opel Rekord P2 Caravan in a metal box. For our Z gauge there is a sliding wall car of the successor type (item no. 800034), also marked for era III.

It is based on the earlier model 8623, but now carries the platforms of this year's new design attached to the roof edge. A car model is again not included with this model. Can we take comfort in the fact that the Minitrix model also has to do without one?

For the LGB Museum Car 2023, the Pinzgauer Lokalbahn from Zell am See (Austria) was chosen as the sponsor. Their operations manager Walter Stramitzer took those present on a detailed historical historical excursion of his railway.

He was very pleased to be represented with the covered freight car from the early forties. He said with a smile that he still had some prototype wagons in his stock, which would certainly be worthwhile for LGB to convert.



And this is what it looks like: museum wagon 2023 for the gauge (item no. 80034) with historic advertising for Stihl power saws. Photo: Oleksiy Mark

A social part with appetisers and above all many discussions at tables and in the aisles rounded off the beautiful evening in Göppingen. Finally, the participants had the chance to take a close look at the museum cars 2023 of all nominal sizes in the showcase.

Free entry for children:

On Christmas Eve, the Miniatur Wunderland in Hamburg is open until 4 pm. In order to give the children of today's generation a similar feeling to their parents, who used to press their noses flat against shop windows to see a model railway layout, they will get free admission up to the age of 15 on 24 December 2022.

Since this cannot be caught in the ticket booking, the following procedure applies: Parents book tickets for all visitors aged 16 and over and omit the beneficiaries. Accompanying children up to 15 years of age will receive their free ticket on site upon arrival at the ticket office. Of course, they are then allowed to enter the exhibition areas together with their family members without having to wait.

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